



**Notice of a public meeting of
Planning Committee A**

- To:** Councillors Cullwick (Chair), Pavlovic (Vice-Chair), Ayre, Barker, D'Agorne, Doughty, Kilbane, Fisher, Looker, Melly and Waudby
- Date:** Thursday, 9 June 2022
- Time:** 4.30 pm
- Venue:** The George Hudson Board Room - 1st Floor West Offices (F045)

AGENDA

1. Declarations of Interest

At this point in the meeting, Members are asked to declare any disclosable pecuniary interest or other registerable interest they might have in respect of business on this agenda, if they have not already done so in advance on the Register of Interests.

2. Minutes (Pages 1 - 6)

To approve and sign the minutes of the last meeting of the Planning Committee held on 5 May 2022.

3. Public Participation

At this point in the meeting members of the public who have registered to speak can do so. Members of the public may speak on agenda items or on matters within the remit of the committee.

Please note that our registration deadlines have changed to 2 working days before the meeting, in order to facilitate the management of public participation at meetings. The deadline for registering at this meeting is 5:00pm on Tuesday 7 June 2022.

To register to speak please visit www.york.gov.uk/AttendCouncilMeetings to fill in an online registration form. If you have any questions about the registration form or the meeting, please contact Democratic Services. Contact details can be found at the foot of this agenda.

Webcasting of Public Meetings

Please note that, subject to available resources, this meeting will be webcast, including any registered public speakers who have given their permission. The meeting can be viewed live and on demand at www.york.gov.uk/webcasts.

4. Plans List

This item invites Members to determine the following planning applications:

a) Development Site Hospital Fields Road and Ordnance Lane York [21/02573/FULM] (Pages 7 - 72)

Demolition of existing buildings, alterations to 'The Married Quarters Building', and erection of new buildings to provide no.85 residential dwellings (Use Class C3), 457sqm of commercial / business floorspace (Use Class E) and 152sqm of community floorspace (Use Class F1/F2) with associated ancillary development [Fishergate Ward]

b) Clifton Park Treatment Centre NHS North Yorkshire and York, Bluebeck Drive, York YO30 5RA [22/00192/FUL] (Pages 73 - 100)

Installation of temporary theatre unit, storage cabin and additional car parking [Rawcliffe and Clifton Without Ward]

5. Planning Appeal Performance and Decisions (Pages 101 - 118)

This report informs Members of planning appeal decisions determined by the Planning Inspectorate between 1 January and 31 March 2022. A summary of each decision is provided in appendix B and a list of outstanding planning appeals in appendix C.

6. Urgent Business

Any other business which the Chair considers urgent under the Local Government Act 1972.

Democracy Officer

Angela Bielby

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For more information about any of the following please contact the Democratic Services Officer responsible for servicing this meeting:

- Registering to speak
- Business of the meeting
- Any special arrangements
- Copies of reports and
- For receiving reports in other formats

Contact details are set out above.

This information can be provided in your own language.

我們也用您們的語言提供這個信息 (Cantonese)

এই তথ্য আপনার নিজের ভাষায় দেয়া যেতে পারে। (Bengali)

Ta informacja może być dostarczona w twoim własnym języku. (Polish)

Bu bilgiyi kendi dilinizde almanız mümkündür. (Turkish)

یہ معلومات آپ کی اپنی زبان (بولی) میں بھی مہیا کی جاسکتی ہیں۔ (Urdu)

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City of York Council

Committee Minutes

Meeting	Planning Committee
Date	5 May 2022
Present	Councillors Fisher (Chair), Ayre, Barker, D'Agorne, Daubeney, Doughty, Douglas, Fenton, Looker, Lomas [from 4.32pm], Melly, Pavlovic (Vice-Chair), Warters, Widdowson (Substitute for Cllr Hollyer) and Hunter (Substitute for Cllr Waudby)
Apologies	Councillors Waudby and Hollyer

81. Declarations of Interest

Members were asked to declare, at this point in the meeting, any personal interests, not included on the Register of Interests, or any prejudicial or disclosable pecuniary interests they may have in respect of business on the agenda. None were declared.

82. Minutes

Resolved: That subject to the addition of Cllrs Melly and Ayre to the list of apologies, the minutes of the last meeting held on 7 April 2022 be approved and then signed by the Chair as a correct record.

[Cllr Lomas joined the meeting at 4.32pm]

83. Public Participation

It was reported that there had been no registrations to speak at the meeting under the Council's Public Participation Scheme on general matters within the remit of the Planning Committee.

84. Plans List

Members considered a schedule of reports of the Head of Planning and Development Services, relating to the following

planning applications, outlining the proposals and relevant policy considerations and setting out the views of consultees and officers.

85. Alton Cars York Ltd, 3 James Street, York YO10 3WW [22/00367/FULM]

Members considered a major full application from Gavin Douglas for the Demolition of existing buildings and redevelopment of the site for purpose-built student accommodation with up to 303 bedrooms, associated communal facilities, car parking and landscaping (resubmission) at Alton Cars York Ltd, 3 James Street, York.

The Case Officer gave a presentation on the application detailing the site layout, elevations, floor plans, top floor plan, massing modelling and a photomontage of the scheme. He then gave a committee update noting comments from Highway Network Management and the Flood Risk Management Team. Amendments to Conditions 5 and Condition 18, the latter to include seating in the landscaping scheme.

Public Speakers

The Applicant, Gavin Douglas, spoke in support of the application. He responded to the reasons for the refusal of the previous application which included a reduced building footprint and increased soft landscaping. He noted the offer of land for adoption for a shared cycle and footway. He explained that the room sizes exceeded requirements.

In response to Member questions, Gavin Douglas explained that:

- The offer of the land for adoption was on the eastern side of the landscaping on James Street. The developer would also fund the highway improvements.
- The shared cycle and footway would be 3.5m and subject to highway officer approval.
- Waste collection would be managed through a private operator. The waste would be sited as far away from properties as possible. There would be a waste management plan. The apartments had segregation units for recycling.
- The means of discharging to the public sewer was explained.

- When asked whether the design had been amended on western side of the site, members were advised that in terms of impact, there was one window on the upper floors on Dunlin House and the west elevation would overlook the communal grounds used either for car parking or as amenity space at Dunlin/Heron House. The windows would be angled away and the building was 22.5m away from the north and 25m away from the south of Dunlin House.
- The windows overlooking the courtyard were semi private.
- Should the application be approved, the appeal for previous refusal of the application would be withdrawn.
- Regarding the communal space, the cluster bedrooms had a shared kitchen and dining space, on the ground floor there was a gym, laundry, cinema and breakout space (which was bookable online).
- The external space was not covered and there was a dedicated route from the cycle store through the external space.
- The applicant was willing to look into e-bike use with Tier.

Members then asked Officers further questions which they responded that:

- Condition 18 could be amended for the landscaping to be for the lifetime of the scheme.
- Regarding the highway improvements, the Applicant would undertake the work under a S.278 agreement and the Council would adopt the land as highway thereafter.
- The application should be determined on its own merits.
- There would be a reference to the frequency of waste collection in the waste management plan and there was scope in Condition 23. This would be a condition and not an informative.
- The response of the Conservation Areas Advisory Panel was confirmed.
- The cycle and footway would be shared, not be segregated.
- The application did not include a scheme for highway works to improve the junction at James Street and Lawrence Street.
- In respect of housing delivery it was not confirmed the 303 bedrooms would translate into as housing units.

Following debate Cllr Fenton moved approval of the application with Condition 18 amended to include the addition of seating and the landscaping to be for the lifetime of the scheme. This was seconded by Cllr Widdowson. Members voted 14 in favour of the motion and 1 against, it was therefore:

Conclusion

Taking into consideration all further comments, aside from the variation to condition 18 (as above), and the officer recommendation is unchanged and no further amendments to the recommendation (as set out in section 7 of the committee report) are proposed.

Resolved: That the application be approved subject to the following amended Condition 18:

Condition 18

The development shall not be occupied until there has been submitted to and approved in writing by the Local Planning Authority a detailed landscaping scheme which shall illustrate the number, species, height and position of trees and shrubs and hard landscaping features including the provision of seating. The landscaping shall be for the lifetime of the scheme.

Reason:

- i. The presumption in favour of sustainable development in paragraph 11d of the NPPF applies to this application. This means permission should be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the policies in the NPPF taken as a whole.
- ii. Officers are content the amendments made to the application address the reasons for refusal of the previous application identified by the planning committee. The concerns of overdevelopment of the site, lack of public realm enhancements and wider benefits are addressed; by the reduced amount of development, increased amount of public amenity space proposed and the junction improvements, combined with wider footpaths, which would make

the area more attractive and pedestrian friendly. It has been evidenced the development would provide adequate levels of amenity for its future occupants (taking into account room/cluster sizes and provision of amenities).

- iii.* NPPF paragraph 123 states Local Planning Authorities “should support proposals to use retail and employment land for homes in areas of high housing demand, provided this would not undermine key economic sectors or sites or the vitality and viability of town centres”. The scheme is acceptable in principle when assessed against the NPPF, taking into account need for the proposed use and the sustainable location. In respect of the former use of the site, the business has relocated to a preferable site. There are favourable sites where the employment needs of the city, over the emerging plan period, can be accommodated. The scheme accords with local policy for the economy and student accommodation.
- iv.* The proposed development ranges in height from 3-5 storey. This building scale is comparable to the development to the west and would not be excessive in this location. There is no harm to the setting of the Central Historic Core Conservation Area. Technical matters are addressed (using planning conditions where necessary). There would be no undue effects on neighbours and a favourable effect on the highway (and setting in this respect) compared to the former use. Approval is recommended; no adverse impacts of the scheme are identified, that would significantly and demonstrably outweigh the benefits when assessed against the policies in the NPPF taken as a whole.

Cllr T Fisher, Chair

[The meeting started at 4.30 pm and finished at 5.17 pm].

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development and further former industrial / warehouse type buildings, accommodating retail and commercial units.

1.4 The site is outside of Flood Zones 2 and 3 and all the buildings on-site are outside of the Conservation Area (as defined in the 2010 Conservation Area Appraisal). The Conservation Area boundary extends only to include the wall that abuts Main Street.

Proposals

1.5 The scheme proposes retention of the Married Quarters building, with other buildings on site demolished to accommodate residential lead development.

1.6 The scheme is part of the Council's Housing Delivery Programme. The proposals are for 85 dwellings (at a density of 65 dwellings per hectare), 8 commercial units, and 2 ground floor units providing community space. 16 trees are proposed for removal (2 category B, others of lower amenity) with up to 145 new trees proposed. The site layout includes a new pedestrian / cycle route through the site and public open space. There would be 10 car parking spaces on the north side of the site, with access via the alleyway behind Ambrose Street. Otherwise there would be 31 car parking spaces on street – within the scheme and on the west side of Hospital Fields Road. The scheme includes provision of a cargo bike hire facility for residents. These would be electric and free to use.

1.7 The dwellings are to be Passivhaus certified, with an aspiration that 100% of domestic energy demand anticipated as provided through on-site renewables.

2.0 POLICY CONTEXT

2.1 National Planning Policy Framework 2021 ('NPPF') key policies / sections are as follows –

- Section 4 Decision making
- Section 5 Delivering a sufficient supply of homes
- Section 9 Promoting sustainable transport
- Section 11 Making effective use of land
- Section 12 Achieving well-designed places

2.2 Publication Draft City of York Local Plan 2018 (2018 eLP) was submitted for examination on 25 May 2018. The plan policies can be afforded weight in accordance with paragraph 48 of the NPPF (taking into account the stage of the plan, the level of objection to policies and consistency of policies with the NPPF).

2.3 Key relevant policies of the 2018 eLP are as follows -

SS1	Delivering Sustainable Growth for York
DP3	Sustainable Communities
H3	Balancing the Housing Market
H10	Affordable Housing
HW7	Healthy Places
D1	Place-making
D2	Landscape and Setting
D6	Archaeology
GI6	New Open Space Provision
CC1	Renewable and Low Carbon Energy Generation and Storage
CC2	Sustainable Design and Construction of New Development
ENV1	Air Quality
ENV3	Land Contamination
ENV5	Sustainable Drainage
T1	Sustainable Access

3.0 CONSULTATIONS

Design, Conservation & Sustainable Development - Architect

3.1 The extent of building demolitions are supported-in-principle, because balancing their losses are factors including: (1) Retention of sufficient good-quality buildings to ameliorate the otherwise stark qualities associated with completely scoured urban regeneration sites. (2) This retention then further grounds the proposals within wider historic fabric. (3) The remaining open site enables development of a more coherent masterplan, at a higher site density than otherwise possible.

3.2 Hard and soft landscape is crucial to the success of the proposal and is attractively designed. The DAS p92 (Design & Access Statement) explains maintenance responsibilities - some areas by CYC adoption and others by resident paid service charges. It is crucial the public realm is constructed extremely robustly, otherwise the cost of rectifying failing surfaces can be crippling.

Layout

3.3 The layout is simple and legible. There are a mix of houses and flats but this is not obvious, considered a positive. The challenges of modern terraced housing design are noted, and the scheme looks to address such, making for a distinctive place. Complex and sometimes conflicting demands include: (1) Servicing for refuse or emergency. (2) Car access and parking. (3) Sufficient attractive outdoor space for safe play and amenity. (4) Privacy of internal and external space. (5)

Medium-density places making additional demands on durability of limited and often multi-functional public realm.

Details

3.4 The proposal has extensive areas of flat roof guarded by multi-sloped high parapets producing the appearance from the ground of a building with a pitched roof. I would normally be reluctant to support this slight disingenuous expression. However, here I support it because it is used in a butterfly configuration, meaning building corners are tall so you can not see behind the “stage set” of the parapet.

Block 5

3.5 There is a 2nd floor external terrace to this proposed building. Upper floor outdoor terraces tend to feel more impactful to neighbour privacy than windows, so it is a shame that this form was chosen for this situation. The study shows how privacy is physically achieved with a set-back handrail/planter, but this might still not be a comfort to those neighbours (on Ambrose Street).

Block 6,

3.6 The proposal comprises a walk-up flat over commercial space. Officer advice was balconies overlooking the neighbouring land to the south be omitted. This has been resolved in revised plans.

Blocks 7 & 8

3.7 The design intervention of the new deck structure is quite radical, but the existing building interventions are pretty modest, externally. This new “armature” also helps tie the existing Married Quarters building into the architectural language of the new that surrounds it- otherwise the building might look a little swamped and out of place.

The idea of the deck is a good one in-theory to help the pragmatics of space planning a building reuse, but external deck access buildings can be prone to creating antisocial or fearful spaces. It isn't particularly clear how this would be avoided here, especially when it is a rear space.

Blocks 9 & 10 fronting Fulford Road

3.8 These buildings underwent significant changes in architectural approach during design development. This version is the most successful. The butterfly roof does not reflect design forms of the conservation area. As an isolated case, this approach would normally not preserve or enhance the characteristics of the conservation area (because it is different it has the potential to erode existing qualities). However, in this instance it is assessed as not harmful. Reasons include: (1) The general material palette and richness of element detailing is appropriately complimentary to the conservation area, so it does not visually jar. (2) The architectural form represents the beginning/gateway of a significant area of redevelopment that follows

a similar approach and so is consistent. (3) The overall high quality of the whole development justifies a certain degree of design risk.

Design, Conservation & Sustainable Development - Archaeology

3.9 An evaluation and watching brief are required. An archaeological evaluation is required in the area adjacent to the bus depot to investigate the continuation of a ditch noted on the site next door in 2007 and to assess the potential of this largely undeveloped area of the site. The evaluation will assist in determining the amount of watching brief required. Ordnance Lane buildings proposed for demolition should be subject to a building recording.

Design, Conservation & Sustainable Development - Landscape

3.10 Support the landscape proposals across the site. However, the open spaces proposed are limited therefore every little bit of planting and landscape detail counts. In this respect, great attention has been paid to the proposed landscape design across the site; utilising every available opportunity and making the multi-functional spaces work hard across a very limited area. The treatment and purpose of each typology is clear, and has a direct and strong relationship with the buildings and the streets. An emphasis on trees, ecology, play, edible plants and shared space, encourages community engagement, which collectively adds to its value. The submitted information does not prove unequivocally that the proposed tree planting is deliverable (and so full details of planting are required through condition).

3.11 Although the development sees the loss of several trees across the site, it aims to incorporate the two most significant trees into the proposed Ordnance Lane pocket park. None of the trees to be removed are exceptional specimens. The proposed landscape scheme includes a good range and distribution of tree species across the development. This provides suitable mitigation that has the potential to provide enhanced tree cover.

Design, Conservation & Sustainable Development - Ecology

3.12 No objection. Recommended conditions.

3.13 The site offers suitable habitat for nesting birds. Therefore, precautionary methods will be needed to ensure active nests are not destroyed during any of the required works.

3.14 Through the provision of integrated bat and bird boxes, as shown on pages 54 and 55 of the Landscape Statement and the inclusion of native planting, as shown on Tree Planting Plan and Schedule, the proposed development will achieve a Biodiversity Net Gain in-line with the National Planning Policy Framework.

Education

3.15 Officers advise contributions are required as follows –

	No. of places	Contribution sought	Location
Primary	12	£227,715	St Oswalds
Secondary	7	£182,882	Fulford phase II
Early years	8	£151,808	within catchment area (up to within 2.5km)

Highway Network Management

Cycle stores

3.16 The amount of cycle storage is significantly above the local minimum standard (of 126 spaces) and is acceptable.

3.17 The cycle stores proposed in private gardens are considered impractical to use (with a side opening, rather than an end door). In dimensions they are (approx.) 2m by 1m. If applying the Council's cycle parking guidance the stores could only accommodate one cycle (two are illustrated). Minimum internal dimensions (for two cycles) are 2m x 1.4m (with end door) in the CYC Cycle Parking Guidance.

3.18 Two-tier parking is discouraged on residential schemes – in response the scheme was amended so the communal stores (in blocks 2 and 4) now provide Sheffield stands and space for over-sized cycles and equipment.

Car parking provision

3.19 Officers undertook a review of typical parking demands for the scheme, based on the house sizes and commercial space proposed and the location. Using 2011 census data the existing number of cars per household in this area is 0.89. This is lower than the figure (1.08) for York. Officers estimate the proposed provision (0.33 spaces per dwelling) will be extremely challenging to provide, without an adverse effect on the highway in terms of displacement car parking in neighbouring areas. They estimate possible demand for between 35 – 22 car parking spaces off-site (the lower figure of 22 assumes car parking remains on Hospital Fields Road on the west side of the site).

3.20 There is space on Hospital Fields Road to park 66 cars on-street, but during the daytime it has approx. 92+% occupancy rate. In the evening occupancy is much lower, leaving space available for residential parking. However the Council have

aspirations to provide a trial segregated cycle facility on Hospital Fields Road to the south of the development – this will likely effect on-street parking.

3.21 Previous parking studies (e.g. for application 19/00603/FULM – Frederick House) have shown that in the vicinity of the site most of the terraced streets to the west of Fulford Road are at, or near, full capacity with limited space to accommodate any additional car parking (particularly Wenlock Terrace, Ambrose Street, Frances Street, etc.). Some of the streets (e.g. Alma Terrace south channel) already have parking restrictions in place and some others towards northern and southern limits of this area (e.g. Grange Garth, Grange Street and Maple Grove) are already in residents parking zones (R20 and R53).

3.22 To address car parking issues officers suggest controlled parking in nearby streets prior to first occupation i.e. a residents parking (ResPark) permit system. Residents of the proposed development would be excluded. A mini ResPark system for the application site is also necessary.

3.23 The costs associated with the two schemes would be as follows –

Traffic Regulation orders

- Off- site ResPark up to £25k
- On site up to £17k

Administration (enforcement of parking restrictions)

- Off site ResPark up to £95k
- On-site up to £2,354

Funding of permits for residents

- Off site ResPark up to £40,400 per year

Electric Vehicle Strategy – project manager

3.24 The facilities proposed - one Rapid bay, one public Fast bay and one car club Fast bay are agreed. The parking bays are to be 3 metres wide to mitigate trailing cable issues in the highway and there will be no obstruction of the footway.

3.25 The site plan only indicates the position of one charge-point. There should be one Rapid charger and one Fast charge-point - there will be space but they are not all shown on the plan.

3.26 The Council's EV strategy team asked to be responsible for determining the final position of the charger and charge-point and other site-specific detail. The installation will be carried out by the Council's appointed installer as they will be part of the Council run York EV Network.

Waste Services

3.27 No objections to waste collection strategy.

Leisure / Public Realm

3.28 There is a shortfall of 4,460qm amenity open space and 504sqm of play space within the scheme. Offsite contributions for amenity open space are required and would be used to in the first instance to increase capacity at Low Moor allotments and for play space, additional play equipment in Rowntree park.

Sports

3.29 The City of York Local Plan Evidence Base: Open Space and Green Infrastructure Update September 2017 shows that the development falls within Fishergate Ward. Fishergate and the connecting ward of Guildhall have a significant shortfall of outdoor sports provision. As such, and as there is no provision on-site, a contribution should be sought.

3.30 The off-site sports contribution is £41,535. Suggest the following clubs and venues as beneficiaries; the Grounds of The Retreat outdoor sports facilities, Rowntree Park Tennis Club, York Canoe Club (based in Rowntree Park), enhancement of existing school playing fields supporting greater community access onto those sites and / or such other provision of off-site outdoor sports provision within the vicinity of the Development.

Public Protection

3.31 Noise – The applicant's noise assessment demonstrates the uses proposed are suitable in this location. The assessment does not finalise the design requirements and the noise mitigation measures so a condition is required to secure such. Details are requested of any plant / machinery equipment and of noise attenuation between floors.

3.32 Construction management – recommend restricted working hours are secured through condition and management measures are approved regarding noise, vibration, dust, lighting and dealing with complaints.

3.33 Land contamination – site investigation and approval / implementation of a remediation strategy (if necessary) recommended due to the former uses of the site.

3.34 Cooking Odour – measures to avoid odour causing amenity issues requested.

3.35 Lighting – measures to avoid light pollution recommended.

Affordable Housing Officers

3.36 A minimum of 40% of the total housing is proposed as affordable, in excess of the policy obligation for a brownfield site of 20%. The affordable housing is of excellent quality. All homes will be to low-carbon “Passivhaus” standard, and at sizes that meet or exceed the good practice Nationally Designed Space Standards guidance. This provision strongly supported by the Housing Policy and Strategy Team.

3.37 The breakdown of housing types for the scheme is as follows:

Dwelling size	Social rent	Shared ownership	Market sale	Total
1-bed flat	7	11	6	24
2-bed flat	3	5	16	24
2-bed house	3	1	4	8
3-bed house	3	0	11	14
4-bed house	1	0	9	10
5-bed house	0	0	5	5
Total	17	17	51	85

EXTERNAL

Conservation Areas Advisory Panel

3.38 The Ordnance Lane site is occupied by former military housing units (including the terrace of former married quarters). All units face away from their respective roads, built to face towards the now demolished hospital building.

3.39 The Married Quarters appear to have been built in 1903 for married hospital staff with the remainder of the houses built later. Retention is proposed whilst other buildings are to be demolished. The justification appears to be twofold: that they are of no particular architectural significance and that the site could be more efficiently developed to provide the required accommodation. Whilst the Panel welcomed the retention, the justification for demolition of other building was weak and based purely upon a need to clear the site.

3.40 The Panel warmly welcomed the principle of the scheme and in general the scale of the development and structures. The revised proposals for the two buildings fronting Fulford Road were considered an improvement. However, there was concern about the somewhat contrived architectural feature of the butterfly roofs.

Designing out Crime Officer

3.41 The principles of crime prevention through environmental design have been incorporated into its design and layout. Only observation is bollard lighting should be avoided as it does not project sufficient light at the right height and distorts the available light due to the 'up-lighting' effect; making it difficult to recognise facial features and as a result causes an increase in the fear of crime. It is also susceptible to deliberate or accidental damage. Lighting for roads and segregated footpaths, regardless of whether adopted or not, should comply with BS 5489-1:2020.

Fishergate Planning Panel

3.42 Raise issue with the limited amount of car parking which is considered inadequate. Reference a lack of visitor parking and parking for the commercial units (neither have dedicated parking spaces identified) and insufficient electric vehicle parking provision. Introducing Resident's Parking on nearby streets will not deal with the resultant issues, which will be displaced to streets further away.

York Civic Trust

3.43 Advise there is much to be supported in the proposals for redevelopment of the site. Reference is made to the ambitions to be a sustainable development with all new houses aspiring to be 'Zero-carbon' and PassivHaus compliant; inter-generational living that can benefit the well-being of all generations involved; low car development, with a strong pedestrian and cycle agenda; the inclusion of communal spaces that are safe for all residents, including children; the prominence and promotion of wildlife in the urban domain, and meeting National Space Standards. The Trust is overall supportive of the principle of the development and the City of York Council / Mikhail Riches partnership proposing high-quality, environmentally-conscious development.

3.44 Further detailed comments were provided on the Fulford Road facing buildings, demolition and the proposed car parking provision.

3.45 In respect of the buildings facing Fulford Road the Civic Trust advise the buildings have real potential to make a positive and unique impact. To achieve such, the inclusion of brick detailing is recommended, to provide architectural interest reflecting the surrounding local vernacular, potentially creating a 21st century take on Victorian brick detailing, of which York has a fine tradition. It was suggested the curved corner leading into the site could incorporate geometric brick detailing and further detail could be included at parapet level to further enhance the building.

3.46 No objection to the demolition of 11-12 Ordnance Lane as it is currently a detractor to the setting of the Married Quarters building and removing will improve

the setting and aesthetics of the building. The loss of the three early 20th century buildings (9-10, 5-8, and 1-4 Ordnance Lane) is lamentable and contributes to the piecemeal erosion of the military heritage of the site. Demolition is at odds with the Council's zero carbon ambitions.

3.47 The Trust welcomes the low-car ambitions of the development and the provisions made for cyclists. However the number of spaces that will be available (41) is just under 50%. This is a well-connected site and this provision it is at odds with the overall sustainability ethos of the development and ambition to be a low car development/community.

Yorkshire Water

3.48 The drainage strategy (in the addendum dated 6.4.22) is acceptable. If permission is to be granted recommend following conditions –

- No piped discharge of surface water from the development until completion of the approved site drainage.
- Measures to protect existing public sewer to be approved prior to construction.
- Easement required to protect public sewer, in accordance with the approved plans.

4.0 REPRESENTATIONS

4.1 Consultation responses are as follows. There have been 23 representations made in objection and 11 in support of the scheme.

4.2 Impact on Wenlock Terrace

- Wider use of access / alleyway to rear of Wenlock Terrace objected to – loss of privacy / noise concerns / devalue properties (10 car parking spaces via the alleyway proposed).
- Loss of trees causes adverse effect on outlook from Wenlock Terrace. T7 (category C tree to the south of proposed block 8) proposed for removal.
- Concern over car parking issues getting worse. Wenlock Terrace should be resident's parking only.
- Security concerns if any changes in boundary treatment proposed within the rear alleyway.

4.3 Impact on Ambrose Street

- Rear yards on Ambrose Street are south facing – concern about loss of light as a consequence of 2 and 3 storey buildings.
- The proposed buildings (block 5) are very close; they should be 2-storey only and further away. There is a concern about overlooking and loss of privacy.

- Unclear whether the wall on the south side of Ambrose Street will be demolished.
- Clarification on future use of back alleyway requested.

4.4 Impact on The Stables

- Loss of light and privacy.
- Impact on amenity due to proximity of access into the application site.

4.5 Objection to demolition

- Although the project includes retention of the 'married quarters' block, the true heritage value of the site lies in retaining all the houses. Retention of some or all of these houses could have provided a positive contribution to the new development as a gateway from Fulford Road, whilst also preserving some of the last remaining evidence of the historic significance of the site.
- Heritage values of the housing is underplayed. The historical merit of the houses is that they were part of the Fulford Road complex of York's military garrison, much of which has been lost and the remainder is under threat of extensive redevelopment.
- The buildings should have been included in the Conservation Area and added to a local list as non-designated heritage assets.
- In the context of the climate emergency, it does not make sense to extinguish the embedded carbon of these houses and burn more carbon creating new buildings.

4.6 Highway Network Management

- Car parking – limited parking on site proposed. Concerns over impact on surrounding streets. It is already very difficult to park in surrounding streets. Objectors note they were in favour of introducing res-park on Ambrose Street (recent CYC consultation). There will be more pressure on parking locally as Alma Terrace is due to become res-park and if this scheme were to go ahead.
- There are multiple representations in support of the scheme because the layout and space provision discourages, and is not dictated by, car parking. Car-free living is cited as crucial in respect of zero carbon and will also enhance quality of life. Schemes such as this must be supported in favour of low-density schemes with higher car parking provision further from the centre.
- Support as the scheme favours active transport options over continually promoting a car-based lifestyle. If we can persuade more people to live car-free, the need for vast amounts of parking space will start to disappear.
- This is an ideal location for a high density and low car development due to the proximity to the centre of York and good transport links. There isn't the local infrastructure to support additional vehicle journeys into this location: Fulford Road already fails many air quality targets and is frequently congested.

4.7 Architecture

Building design not in-keeping with the surrounds.

- Support for beautiful design, human scale architecture to passive house standards; low-car design that is entirely in keeping with a neighbourhood close to the city centre and other amenities, with good bus and cycle routes.
- Support for the overall design which places a high emphasis on community and social interaction.
- It is encouraging to see excellent accessible properties included, a critical feature as York has an appalling absence of properties of this type.
- Support for the provision of high quality accessible housing and multi-generational house types. Such properties are hard to find in York.
- Support for high energy efficiency of the dwellings proposed.

4.8 York Cycle Campaign

- Would like to see the number of cargo cycle spaces increased to 14 or more (at least 5% of the cycle parking), in line with Government guidelines (LTN1/20 – which state “a proportion of the cycle parking (typically 5%) should be provided for non-standard cycles to accommodate people with mobility impairments”).
- The width of the cycle parking is tight (1m for a two-cycle store). This is the minimum recommended width; would like to see additional width given to the cycle stores, to ensure that it is easy to store cycles and thereby encourage use.
- If car parking provision were altered on Hospital Fields Road ask that the opportunity is taken to install a segregated cycle path on the pavement side of the car-parking, to avoid 'car-dooring' incidents, and in line with national cycle infrastructure guidelines.

4.9 Cllr D Taylor

- The architecture is acceptable, and amendments to the elevations onto the main road are an improvement from the original drawings. The front elevations could be more impressive, taller, and mimic the Victorian properties neighbouring the site, albeit in a modern idiom. Must be active frontages onto the main Fulford Road.
- The historic buildings on the site should have been included in the Conservation Area – suspect they were not because of the Council's intent to re-develop the site. The Married Quarter must be retained (retention is proposed).
- Support for Passivhaus approach and commercial and community spaces proposed.

4.10 York Environment Forum

- Support the scheme due to the contribution to carbon reduction (including from use of renewable energy, highly insulated dwellings, measures to reduce private

car use, ecological impacts, and provision of an environment that encourages social interaction, play and recreation.

5.0 APPRAISAL

5.1 Key Issues

- Principle of the proposed development
- Accommodation mix
- Design and impact on the Fulford Road Conservation Area
- Sustainable design and construction
- Impact on surrounding residents
- Highway Network Management
- Open Space
- Ecology
- Drainage
- Education
- Public Protection

Principle of the proposed development

5.2 The site currently accommodates a bus depot, used by Transdev, a tool sales/hire company and former temporary housing operated by the Council. Transdev intend to relocate to another council owned site by the Rawcliffe Bar Park and Ride (planning permission approved under application 22/00400/FUL). The application site is not allocated in 2018 eLP; part is regarded as employment land.

5.3 The presumption in favour of sustainable development in paragraph 11d of the NPPF applies to this application. This means permission should be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the policies in the NPPF taken as a whole.

5.4 The Local Planning Authority cannot demonstrate a current NPPF compliant five year housing supply. It has been required (see NPPF paragraph 76) to prepare an action plan to improve delivery. The Council's action plan relies substantially on the Housing Delivery Programme. As such, and given the importance of housing delivery in the NPPF, the scheme is supported in principle. In this context -

- NPPF paragraph 60 sets out the Government's objective of "significantly boosting the supply of homes, it is important that a sufficient amount and variety of land can come forward where it is needed, that the needs of groups with specific housing requirements are addressed and that land with permission is developed without unnecessary delay".

- Paragraph 123 states “Local planning authorities should also take a positive approach to applications for alternative uses of land which is currently developed but not allocated for a specific purpose in plans, where this would help to meet identified development needs. In particular, they should support proposals to: use retail and employment land for homes in areas of high housing demand, provided this would not undermine key economic sectors or sites or the vitality and viability of town centres, and would be compatible with other policies in this Framework”.

5.5 In respect of employment land NPPF paragraph 82 establishes local plan requirements to set out a clear economic vision and strategy which positively and proactively encourages sustainable economic growth and to set criteria, or identify strategic sites to meet anticipated needs over the plan period.

5.6 Policy EC1 of the 2018 eLP identifies need for each type of employment use over the emerging plan period and allocates sites which more than provide land to meet demand.

5.7 Policy EC2 relates to any site which involves the loss of employment land. For compliance it requires

- i. the existing land and or buildings are demonstrably not viable in terms of market attractiveness, business operations, condition and/or compatibility with adjacent uses; and
- ii. the proposal would not lead to the loss of an employment site that is necessary to meet employment needs during the plan period.

5.8 Part i of the above criteria has not been fulfilled. Non-compliance with this part of the local policy is given limited weight in assessing the scheme overall because criteria ii is satisfied and as the implications are outweighed by the following benefits of the scheme -

- The scheme would provide employment uses - commercial and community units (10 units overall providing 609 sq m floorspace).
- Policy EC1 identifies employment land to meet need over the plan period, as required by the NPPF, which does not include this site.
- Housing delivery in the context of need (including affordable housing need) and the weight attributed to such in the NPPF.
- The positive approach set out in NPPF paragraph 123 to re-using employment land to meet housing need.
- The multiple other social and environmental benefits of the scheme (including public open space, biodiversity net gain and pedestrian cycle links) set out in the following paragraphs of this appraisal.

Criteria ii is met as sites allocated within the 2018 eLP are intended to reasonably meet need, as explained in policy EC1; the class B8 (storage and distribution uses)

have been replaced at the Rawcliffe site (see 5.2) and the scheme would provide employment uses (see point 1 above).

Accommodation Mix

5.9 The mix of accommodation proposed is compliant with relevant local policy. 2018 eLP policy H3 states proposals for residential development will be required to balance the housing market by including a mix of types of housing which reflects the diverse mix of need across the city. This includes flats and smaller houses for those accessing the housing market for the first time, family housing of 2 to 3 beds and homes with features attractive to older people.

5.10 The accommodation mix proposed is below.

24 x 1-bed 29%
32 x 2-bed 37%
14 x 3-bed 16%
10 x 4-bed 12%
5 x 5-bed 5%

5.11 At least 40% of the dwellings are to be affordable. This is an increase over the 20% target in 2018 eLP policy H10. Given affordable housing need and the lack of a 5-year housing land supply this is a benefit of the scheme that has to be given substantial weight in considering the application. All dwellings are dual aspect and meet the (optional) nationally prescribed space standards. In terms of accessibility, 7 of the dwellings would be M4(3) category and 55 M4(2). These are Building Regulations standards described as follows -

- M4(2) dwellings are required to meet the needs of occupants with differing needs, including some older or disabled people; and to allow adaptation of the dwelling to meet the changing needs of occupants over time.
- M4(3) dwellings allow simple adaptation of the dwelling to meet the needs of occupants who use wheelchairs; or (b) meet the needs of occupants who use wheelchairs

Design and impact on the Fulford Road Conservation Area

Policy context

5.12 NPPF para. 126 states the creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. It goes on to state in para. 134 that development that is not well designed should be refused. The fundamentals of good design, which planning should achieve, are set out in para 130, summarised as follows -

- Function well and add to the overall qualities of the area
- Be visually attractive as a result of good architecture, layout and appropriate and effective landscaping;
- Be sympathetic to local character and history while not preventing or discouraging appropriate innovation or change (such as increased densities);
- Be distinctive
- Optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development
- Create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

Impact on the conservation area and appraisal of Ordnance Lane buildings proposed for demolition

5.13 The Fulford Road Conservation Area was designated in 1975. The only development proposed within the Conservation Area is demolition of the wall (with railings) that runs alongside Fulford Road. The 2009 Fulford Road Conservation Area Appraisal report considered a change to the conservation area boundary and inclusion of Ordnance Lane. It advised “the buildings to the rear (i.e. the Married Quarters which it is proposed to retain) have some quality and architectural interest, as well as some social interest. The area would not justify conservation area status in isolation but in association with the Wenlock Terrace housing it is considered to add further interest and diversity, and to justify inclusion”. However despite the report recommendation Ordnance Lane was not included within the conservation area.

5.14 In considering the impact of proposals on designated Heritage Assets NPPF para. 195 states “Local planning authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage asset)”. It goes on to advise, in para.197 that in determining applications, take into account the desirability of new development making a positive contribution to local character and distinctiveness.

5.15 The boundary wall is some 1m tall with railings above. It is terminated by a pillar at the entrance to Ordnance Lane which is in contrasting brickwork and dated 1940. The wall itself has been subject to repairs and its brickwork is therefore varied. Public space (including cycle parking by the commercial units) is proposed in this area. The setting will be attractive and given the low quality and low historic importance of the wall, the alterations will not harm the conservation area.

5.16 The group of buildings along Ordnance Lane were constructed (between 1892-1930) to be used in connection with the Military Station Hospital, which occupied the land to the south of Ordnance Lane - constructed around 1854, demolished in the

late 20th century. The scheme proposes demolition of existing buildings apart from the 'Married Quarters'. The buildings are not designated heritage assets and are not within a conservation area. Of the group of buildings, the one proposed for retention is regarded as that of most architectural and social interest. Overall, the benefits of the scheme, when assessed against the NPPF, outweigh the case for the retention of buildings proposed for demolition.

Design Appraisal

5.17 The scheme comprises of a layout and composition of uses which would add to the qualities of the area. There is enhanced connectivity as the new Ordnance Lane will provide a pedestrian and cycle route through the site (not the case previously) leading to the amenities in the Hospital Fields area and connecting to the riverside. The route will be attractive and provide access to new public open space and small-scale community and commercial buildings within the site. The scheme proposes a significant number of additional trees overall (over 100) and an enhanced biodiversity value for the site. Each provision is of local benefit, in the interests of health and well-being, and weighs in favour of the scheme.

5.18 Demolition is proposed for the buildings of least architectural value on site because of their condition, efficiency and plan form. The scheme enables change which allows for innovation; providing homes with very high environmental standards i.e. Passivhaus, with layouts that adhere to the optional national space standards and enables a more efficient and higher density development. The approach conforms with the design recommendations in NPPF paragraph 130 (in 5.12).

5.19 The house types are of a similar theme to others within the Housing Delivery Programme. The buildings are a mix of two and three storey and predominantly of red brick, with decorative detailing, all of which respect the setting. There are a mix of building types, in terms of architecture and use along Fulford Road. The existing building on site fronting Fulford Road, is primarily orientated south; its main orientation is not towards the street and it is diminutive in scale. The existing building has unsympathetic extensions and does not address the street in the same manner as the remainder of buildings on this side of the street within the Conservation Area (identified in the area appraisal as being of "positive value" or are Listed Buildings), taking into account orientation, building line and layout and scale. The proposed frontage buildings are a mirrored pair of 3-storey buildings with butterfly roof. The design has been subject to multiple iterations and pre-application discussions with input from the Design and Conservation team. Officers are satisfied with the scheme (as advised in paragraph 3.8). The scheme is mindful of local advice in policy D1: Place-making which asks that developments are not a pale imitation of past architectural styles. The proposed buildings, and specifically the roof form, was arrived at taking into account the principles of buildings not appearing as a lesser-scaled imitation of the imposing Wenlock Terrace, allowing the scale of

the terrace to remain dominant in the street scene. Visually it also gives a coherent appearance to the entire scheme, adding to local distinctiveness. The building addresses the street and the commercial uses at ground floor (which turn the corner, leading into the site) will help entice people into the site (rather than giving the impression of a residential only community). Officers are content the scheme would not harm the setting of the Fulford Road Conservation Area.

Secure by Design

5.20 Crime prevention has informed the scheme and the Police Architectural Liaison Officer only commented on the low-level lighting (which can be addressed through condition). Key measures are that public spaces all have good natural surveillance. Public and private space is evident due to boundary treatment. The communal courtyards between the terraces have access control and provide secure space for residents. There would be boundary treatment between amenity space in the scheme and the alleyway to the north.

5.21 The apartments at the married quarters and the neighbouring block (blocks 7 & 8) have an external access at first floor on the north side of the buildings. There is reference in paragraph 3.7 to the communal access areas as potentially being anti-social or fearful spaces. The concern is due to the lack of natural surveillance with the area being at the rear / north side of the site. The access deck enables inclusive access to the upper floor flats and is located to preserve the main south facing elevation of the Married Quarters. The applicants design and access statement contains a summary of secure by design measures. The decks will have access control systems, as required by secure by design guidance. An external lighting plan is included in the submission. This includes average levels, which are reasonable for the area type (as defined in Institute of Lighting Engineers Guidance). Communal cycle stores within outbuildings are provided with additional lighting and main streets through the site are provided with 3m high lighting columns (as recommended by the police). There will be reasonable measures in respect of deterring crime, including access control, building security and lighting.

Sustainable design and construction

5.22 The scheme is within the Council's Housing Delivery programme and all new build dwellings will be to Passivhaus standards. The Married Quarters, being retained and refurbished, will not be as energy efficient as the new buildings. The scheme will incorporate low / zero carbon technology - mechanical heat and ventilation and PV panels. The intention is the PV panels provide almost all of the anticipated energy requirements of the buildings. The proposals for the scheme will exceed the local requirements for carbon reductions in policy CC1 of the 2018 eLP.

Impact on surrounding residents

5.23 The impact on the neighbouring houses to the north - Ambrose Street and Wenlock Terrace is deemed acceptable, as explained below. Also, multiple trees are proposed adjacent the north boundary, which will be 'extra heavy' standard when planted. There is also a block of development referred to as The Stables between these two terraces. The scheme proposes soft landscaping / public open space opposite The Stables and would have no adverse effect.

Ambrose Street

5.24 Block 5 would be to the south of, and parallel to, the terrace of houses on Ambrose Street. The proposed block is predominantly 2-storey. Only the houses at each end step up, to 2.5-storey (with the top floor set further back from the north boundary at house 10) and 3 storey at the opposite end. The 3-storey dwelling has a separation of 21m from the main rear elevation of the houses opposite. The separation decreases down to just over 19m at the other end of the proposed terrace. The lesser separation distance is comparable with back-to-back distances established locally, for example between Frances Street and the north side of Ambrose Street.

5.25 Block 5 is predominantly 2-storey (lower in height than Ambrose Street and other more dominant local buildings). Separation distances between terraces would reflect what is typical for the area, in accordance with NPPF design policy to respect local character (considering scale and layout). The proposed buildings would not be unduly dominant or over-bearing.

5.26 The house at the east end of block 5 (house 10) has outside amenity space proposed at first floor and top floor levels. The outside spaces have privacy screens (1.8m high) supplemented with planting to prevent overlooking over Ambrose Street. As such and taking into account the separation distances achieved, there would be no undue overlooking / loss of privacy.

5.27 The BRE Site layout planning for daylight and sunlight: a guide to good practice has been applied, and its 25 degree guide, which determines that the houses on Ambrose Street would not suffer an undue impact in respect of both daylight and sunlight. The guide is applied where buildings are directly facing windows (i.e. the rear elevation of Ambrose Street; not side windows in rear extensions). A further technical study undertaken by the applicant's consultants, again following BRE guidance, has also been issued which evidences that outside amenity space at the rear of houses on Ambrose Street would experience no material impact on receipt of daylight and sunlight. The proposed buildings would cause no further overshadowing to rear yards than existing outbuildings and boundary walls.

Wenlock Terrace

5.28 Blocks 7 and 8 are proposed to the south of Wenlock Terrace. Block 7 being the retained Married Quarters building.

5.29 Block 7 has communal and commercial units at ground floor level and therefore a new external access staircase and deck at first floor level (the latter connecting to the lift in block 8) are proposed on the north side of the buildings, to provide access into the upper floor residential. Block 8 also has outside amenity space at 2nd floor level facing north, towards Wenlock Terrace.

5.30 The access deck and external amenity space on the north side of proposed blocks 7 and 8 is between 19m – 22m from the main rear elevation of Wenlock Terrace. The outside space on block 8 has a reasonable degree of enclosure. The buildings on Wenlock Terrace are sub-divided into apartments; the rear courtyard areas are communal and provide ancillary storage space. The proposed outside amenity spaces do not have privacy screens, as proposed on the outside amenity space opposite Ambrose Street. Given the scale and use of Wenlock Terrace, it is considered the external access deck would not lead to undue overlooking / loss of privacy.

5.31 Block 8 would be 3-storey, comparable in height to the retained Married Quarters building and lesser in scale compared to Wenlock Terrace. It would not be overbearing or over-dominant.

Highway Network Management

5.32 NPPF para 111 establishes that refusal on highways ground is only sustainable if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

5.33 NPPF paragraphs 110 and 112 advise when considering development proposals

- Appropriate opportunities to promote sustainable transport modes can be – or have been – taken up.
- Highway design reflects current national guidance, including the National Design Guide and the National Model Design Code (topic covered in design section).
- Any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree.
- Give priority to walking and cycling.
- Address the needs of people with disabilities and reduced mobility in relation to all modes of transport.
- Create places that are safe and allow for servicing and emergency access.
- Enable charging of plug-in and other ultra-low emission vehicles in safe, accessible and convenient locations.

5.34 Considering NPPF requirements the scheme is not objected to on either highway safety grounds or considering effects on the highway network. In respect of the low car parking provision there is deemed to be suitable mitigation; the promotion of and ability for sustainable modes of travel and provision for resident's parking schemes in the area.

Impact on the network

5.35 There would be 85 dwellings and 28 car parking spaces on-site. The 10 spaces behind blocks 7 and 8 would be controlled by the applicant/developer and priority given to disabled persons. The other spaces would all be on-street (and therefore a resident's parking scheme would be required to manage their use). A further 10 spaces can be accommodated on-street on Hospital Fields Lane (to the west of the site, not to the south) and 3 further spaces that would be associated with the electric vehicle charging facility proposed and car club. This equates to 41 spaces overall – 48% provision.

5.36 NPPF advice on parking standards is they can be informed by – location and accessibility, public transport, type of development proposed, local car ownership levels and the need to provide adequate facilities for electric vehicles.

5.37 In respect of the NPPF criteria, the site is within an acceptable walking distance of the city centre and services to cater for residents day-to-day needs and there is sufficient public transport on Fulford Road. However 39 of the 85 dwellings would have 3 or more bedrooms. Local statistics for the area (2011 census figures for Fishergate) show some 0.9 car ownership per dwelling.

5.38 Highway Network Management have concerns that, without mitigation based on the house types proposed and car ownership levels locally, there is a risk future residents would look to park off-site causing disruption to the network. There are objections from local residents, specifically along Ambrose Street, to this effect. Letters in support welcome the scheme because it discourages car use in what is a suitable location for a low-car development given accessibility to amenities and services and taking into account air quality on Fulford Road.

5.39 The recommendation from Highway Network Management is that a min-residents parking scheme be introduced at the site, this would control parking and make allowances for visitors and deliveries. They also deem it necessary the developer funds the costs associated with facilitating the introduction of Residents Parking in surrounding streets to address the possible issue of residents of the proposed scheme parking in such areas.

5.40 The projected full costs of the residents parking processes are -

Implementation of a residents parking scheme within the site and funding measures towards administration.

- £17k (costs to include Traffic Regulation Order associated with establishment and administration (enforcement of parking restrictions – street patrol officers / admin of fines and challenges to fines / responding to phone calls regarding cars parked without permits (over a 5-year period)).

Implementation of a residents parking scheme in the surrounding area.

- Traffic Regulation Order - £25k (allowing for two rounds of consultation for implementation of a residents parking scheme, the first prior to first occupation, the second within 1 year of 90% occupation). This would not be required if the res-park comes into force regardless.
- Administration (enforcement of parking restrictions – street patrol officers / admin of fines and challenges to fines / responding to phone calls regarding cars parked without permits (over a 5-year period)) - £95,428.
- Funding of residents permits - £40,400 (per-year membership for residents).

Promotion of sustainable travel / mitigation of effects on the network

5.41 The Council is already due to go out to re-consultation in respect of introducing residents parking to the streets to the north of the site. This consultation includes Frances Street, Ambrose Street, Holly Terrace, Carey Street and Wenlock Terrace (agreed at Executive Member for Transport Decision Session on 19.4.2022). There is already agreement for introducing resident's parking at Kilburn Road, Alma Terrace and Alma Grove. There is already resident's parking at Maple Grove to the south.

5.42 It is recommended that should resident's parking not be taken forward following the process set out above, then another consultation, if/when this housing scheme is complete, would be funded by the developer. There would also be resident's parking at the application site. These measures will control parking locally. Officer advice is that requiring the developer to fund the administration (over a 5-year period) of the off-site scheme would not meet the NPPF tests in respect of planning conditions and obligations. The full administration of the neighbouring residents parking area would not be directly related to this development and nor would it be fairly and reasonably related in scale and kind. Also, the Council has already secured a planning obligation for establishment of residents parking zones locally through the Frederick House development (19/00603/FULM) and in accordance with planning guidance the Council would need to evidence obligations have been spent on the infrastructure referenced or returned.

5.43 In addition to controlling car parking, the main principles of the Council's Housing Delivery Programme (of which this scheme is part of) include supporting

sustainable transport, reducing environmental impact and building healthy neighbourhoods. Low car use is a fundamental component. To this effect the layout is designed to promote wider connectivity through sustainable modes of travel and provide a setting not dominated by cars. The scheme will be marketed on the basis of low car ownership and sustainable travel modes encouraged through the travel plan, provision of on-site facilities for cycles, a scheme for free electric cargo bike hire and car club (vehicle provided at the site and discounted membership for residents). Prioritising active travel is a requirement of the National Design Guide and this scheme is compliant.

Cycle parking provision

5.44 Locally minimum cycle standards are 1 space for a 1 or 2 bed sized dwelling; 2 spaces for larger dwellings. LTN 1/20 is national guidance which recommends 1 space per bedroom and 5% of provision to be for non-standard cycles.

5.45 The scheme would provide 195 bedrooms. Some 250 cycle parking spaces are proposed that are covered and secure. Additionally there are 16 external visitor spaces, spaced over the site and 8 spaces for cargo bikes. The provision is acceptable. Highway Network Management note that the stores which are in front and rear gardens of terraced houses would only fit 1 cycle if applying local guidelines. If this were the case, the provision would reduce to 220 spaces. This remains in excess of the LTN 1/20 recommendation. However it is noted that stores of comparable dimension (approx. 2m x 1m) and design are typically quoted by manufacturers as able to accommodate 2 – 3 cycles.

5.46 LTN 1/20 recommends 5% provision be for non-standard cycles. In addition to the 8 cargo bike spaces the communal stores within blocks 2 and 4 also have space for over-sized cycles (which would take the provision beyond 5%). The homes in blocks 1-5 all have private yard areas which could provide such storage space.

Highway design and priority for walking and cycling.

5.47 The National Design guide refers to patterns of movement that encourage activity and social interaction and that incorporate green infrastructure.

5.48 The scheme contains a public realm design which promotes health and well-being, recreation, walking and cycling. The layout of streets and spaces improves wider connectivity for pedestrians and cyclists (which is essentially car-free), between Fulford Road, the amenities to the south of the site and the recreation facilities and walking and cycling routes by the riverside.

Mobility

5.49 The scheme takes mobility reasonably into consideration.

- Access into dwellings - all buildings have level access and are Building Regulation compliant in respect of access.
- Allocation of car parking spaces – priority will be given to blue badge holders.
- Communal cycle stores have space for over-sized cycles.

Safe access and provision of servicing

5.50 Details have been provided that illustrate waste collection and emergency vehicles can fully access the site. Emergency access is from either Main Street or via Hospital Fields. Waste services have no objections to the servicing arrangements.

EV provision

5.51 NPPF requirements are to enable charging of plug-in and other ultra-low emission vehicles in safe, accessible and convenient locations. For cars one Rapid charger and one Fast charge-point are proposed, to be installed as part of the Council's EV network programme (the car club space will also be served by an ev charging point). These facilities will be for public use and have been agreed with the Council's EV Strategy officers. The four cargo bikes to be provided for hire will be electric also.

Open Space

5.52 The NPPF advises that planning decision should aim to create healthy and inclusive places. Paragraph 98 states 'access to a network of high quality open spaces and opportunities for sport and physical activity is important for the health and well-being of communities. Planning policies should be based on robust and up-to-date assessments of the need for open space, sport and recreation facilities (including quantitative or qualitative deficits or surpluses) and opportunities for new provision. Information gained from the assessments should be used to determine what open space, sport and recreational provision is needed, which plans should then seek to accommodate'.

5.53 Policy GI6 (new open space provision) of the 2018 eLP states 'all residential development proposals should contribute to the provision of open space for recreation and amenity'.

5.54 Compared to local guidance for space standards there is a shortfall in proposed provision of 4,460qm amenity open space and 504sqm of playspace). There is no on-site sports provision and therefore an off-site contribution is required. The contributions would be committed at the locations specified in paragraph 3.28 & 3.30.

5.55 Off-site contributions required (rounded) -

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Amenity space	£16,610
Play	£55,230
Sports	£41,535

Ecology

5.56 NPPF paragraph 174 states decisions should contribute to and enhance the natural and local environment by minimising impacts on and providing net gains for biodiversity (10% is national best practice and due to become mandatory).

5.57 Some 17 trees will be removed to accommodate the scheme. Of these two are category B (one located on the south east side of the site on the corner with Hospital Fields Road and one by the group of trees outside of the site, to the south of the married quarters), the others are category C or U. The layout allows for retention of trees on site of the highest value (see landscape officer comment in 3.11). In terms of amenity value both Category B trees to be removed are adjacent retained Category B trees. Over 100 trees will be planted as part of the proposals.

5.58 The proposals include the submitted Landscape Management and Maintenance Report and Landscape Statement, which set out aspirations for the scheme in respect of biodiversity value. A detailed planting schedule commits to planting multiple 'extra heavy' standard trees. The measures would deliver biodiversity net gain on-site, as required by the NPPF. In landscape and biodiversity terms, the benefits of the scheme outweigh the loss of 16 trees proposed to accommodate the scheme. Full details of tree planting will be required to ensure trees have sufficient soil volumes and protection, where in close proximity to hard-standing. It is noted that due to the Yorkshire Water drainage easement between blocks 3 and 4 tree planting will be constrained in this area.

Drainage

5.59 Relevant NPPF advice on flood risk is not to increase flood risk elsewhere. With regards to drainage developments should incorporate sustainable systems (following the hierarchy in national planning guidance), unless there is clear evidence that such would be inappropriate. 2018 eLP policy ENV5 requires surface water run off at brownfield sites is restricted to 70% of the existing rate, where practical.

5.60 In respect of the sustainable drainage hierarchy, site investigations suggest that ground conditions mean soakaway will not be feasible. Drainage will connect into the existing network. The connection and run-off rates will accord with local policy and be as required by the Council's Flood Risk Management Team. Surface water will be restricted to 70% of the existing (proven) rate, retained in over-sized pipes on-site before connecting into the network. In accordance with best practice,

connection will be into the surface water sewer (the existing connects into the combined sewer). The site-specific details and arrangements for future maintenance will be required through planning condition.

Education

5.61 NPPF paragraph 95 states that it is important that a sufficient choice of school places is available to meet the needs of existing and new communities. Officers have advised contributions would be required for primary, secondary and early years places. The table in section 3 identified the no. of places, contributions required and where they would be committed.

Public Protection

5.62 NPPF policy on land contamination and noise is set out in section 15 of the NPPF. There will be a condition to deal with construction management, due to the scale of the development, proximity to residential and as demolition is involved.

5.63 Noise – the dominant noise source was road traffic (rather than from any surrounding commercial premises) and the submitted noise assessment illustrates that with mitigation (double glazed windows in this case) noise levels within dwellings would be acceptable. A condition proposes approval of a detailed scheme for each block.

5.64 Land contamination – standard conditions recommended regarding a site investigation and implementation of a remediation strategy.

6.0 CONCLUSION

6.1 The social and environmental objectives of the NPPF are as follows –

- **social objective** – to support strong, vibrant and healthy communities, by ensuring that a sufficient number and range of homes can be provided to meet the needs of present and future generations; and by fostering well-designed, beautiful and safe places, with accessible services and open spaces that reflect current and future needs and support communities' health, social and cultural well-being; and
- **environmental objective** – to protect and enhance our natural, built and historic environment; including making effective use of land, improving biodiversity, using

natural resources prudently, minimising waste and pollution, and mitigating and adapting to climate change, including moving to a low carbon economy.

6.2 The scheme meets these objectives. It proposes residential lead development within a setting designed to encourage recreation and social interaction, taking into consideration the public realm and the community uses proposed within the buildings. 40% of the homes will be affordable. In respect of health and moving to a low carbon economy the proposed homes will exceed optional national space standards and target Passivhaus standards that latter significantly exceeds local or national energy efficiency requirements. There will biodiversity net gain on-site and a significant increase in the number of trees on-site. These benefits weigh strongly in favour of the scheme and justify the demolition proposed.

6.3 The NPPF requires, as set out in paragraph 11d, this development should be approved unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies the Framework taken as a whole.

6.4 The adverse effects identified are the demolition of existing buildings and potential effects on surrounding streets in respect of car parking. The demolition is justified due to housing need the environmental quality of the proposed homes and a reasonable scheme of mitigation has been identified to manage on street car parking. There are no adverse effects which justify refusal of the scheme.

6.5 Conditions will secure provision of the following items, which would normally be secured through a planning obligation, because the council is the landowner.

- Affordable housing
- Education
- Open space and sports provision
- Residents parking – funding for Traffic Regulation Orders for residents parking at this site and towards establishment at residents parking on streets to the north

7.0 RECOMMENDATION: Approve subject to the following planning conditions -

1 Time

The development shall be begun not later than the expiration of three years from the date of this permission.

Reason: To ensure compliance with Sections 91 to 93 and Section 56 of the Town

and Country Planning Act 1990 as amended by section 51 of the Compulsory Purchase Act 2004.

2 Approved plans

The development hereby permitted shall be carried out in accordance with the following plans:-

Site Location Plan (EX) 002 P3

Proposed Site Plans

Site Plan 001 P4

Roof Plan 002 P1

Demolition Plan 003 P1

Proposed Site Elevations

Site Elevations A, F, G 220 P1

Site Elevation B-B 221 P1

Site Elevation C-C 222 P1

Site Elevations D & E 223 P1

Site Elevations H224 P2

Proposed Block Plans

Block 1 GA Plans GF & 1F 101 P1

Block 1 GA Plans 2F & Roof Plan 102 P1

Block 2 GA Plans GF & 1F 103 P1

Block 2 GA Plans 2F & Roof Plan 104 P1

Block 3 GA Plans GF & 1F 105 P1

Block 3 GA Plans 2F & Roof Plan 106 P1

Block 4 GA Plans GF & 1F 107 P1

Block 4 GA Plans 2F & Roof Plan 108 P1

Block 5 GA Plans GF & 1F 109 P2

Block 5 GA Plans 2F & Roof Plan 110 P2

Block 6 GA Plans GF, 1F, Roof Plan 111 P2

Block 7 GA Plans GF & 1F 112 P1

Block 7 GA Plans 2F & Roof Plan 113 P1

Block 8 GA Plans GF & 1F 114 P1

Block 8 GA Plans 2F & Roof Plan 115 P1

Block 9 & 10 GA Plans GF & 1F 116 P1

Block 7 & 10 GA Plans 2F & Roof Plan 117 P1

Proposed Site Sections

Site Sections A-A, B-B 320 P1

Site Sections C-C, D-D, E-E 321 P1

Proposed Block Elevations

Block 1 Elevations	201 P1
Block 2 Elevations	202 P1
Block 3 Elevations	203 P1
Block 4 Elevations	204 P1
Block 5 Elevations	205 P2
Block 6 Elevations	206 P2
Block 7 Elevations	207 P1
Block 8 Elevations	208 P1
Block 9 Elevations	209 P1
Block 10 Elevations	210 P1

Proposed House Types Plans and Elevations

House Type A	120 P1
House Type B	121 P1
House Type C	122 P1
House Type D	123 P1
House Type E	124 P1
House Type F Plans	125 P2
House Type F Elevations	126 P2
House Type H	128 P1
Type I GF / 1F	129 P1
Type I 2F / Roof	129.1 P1

Other Site Structures Details

Typical Back Garden Bike & ASHP Stores	250 P1
Typical Cargo Bike Store	251 P1
Bin Stores -	252 P1, 253 P1, 254 P1, 255 P1, 256 P1, 257 P1, 258 P1
Sub Station Detail	259 P1

Landscape Plans

Landscape Plan P13	L-S-001-21218-PGA01
Tree Planting Plan	P06
Hard Surfacing Plan	P05

Other Plans

Tree Constraints Plan	1349 Rev 1
Site External Lighting Plan	E-010 P3
Site Utilities Plan - Proposed	ME-001 4

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

3 Affordable Housing

No development shall commence unless and until a scheme for affordable housing provision has been submitted to and approved in writing by the Local Planning Authority. The affordable housing shall thereafter be provided in accordance with the approved scheme.

The scheme shall include:

- The amount type / tenure and location of the affordable housing.
- The timing of the construction of the affordable housing.
- The arrangements to ensure that such provision is affordable for both initial and subsequent occupiers of the affordable housing.
- The occupancy criteria to be used for determining the identity of prospective and successive occupiers of the affordable housing, and the means by which such occupancy shall be enforced.

Reason: In order to meet identified need for affordable housing in accordance with policy H10 of the Publication Draft Local Plan 2018.

4 Education

No development shall commence unless and until a scheme to ensure the provision of adequate additional early years / pre-school (funding for 8 places), primary (12 places) and secondary (7 places) facilities within the local catchment area, or alternative arrangements, has been submitted to and approved by the local planning authority. The facilities shall thereafter be provided in accordance with the approved scheme, or the alternative arrangements agreed in writing by the Local Planning Authority prior to first occupation of the development.

Reason: The education provision within the catchment area of the development has insufficient capacity to take more pupils, such that additional places are required in the interests of the sustainable development of the city. The provision is required in accordance with NPPF paragraph 94, Policy DM1 of the Publication Draft Local Plan 2018 and the Council's Supplementary Planning Guidance "Developer Contributions to Education Facilities" dated June 2019 update.

5 Open Space and Sports Provision

No development shall commence unless and until details of provision for off-site public open space and sports facilities, or alternative arrangements, have been submitted to and approved in writing by the Local Planning Authority. The facilities shall thereafter be provided in accordance with the approved scheme, or the alternative arrangements agreed in writing by the Local Planning Authority prior to first occupation of the development.

Reason: In order to comply with the provisions of Policy G16 of the Publication Draft Local Plan 2018 which requires that all new housing sites make adequate

provision for the open space needs of future occupiers.

6 Tree protection

Trees shown as retained on the approved plans shall be protected in accordance with BS: 5837: Trees in relation to construction and details within the Quants Arboricultural Impact Assessment 1349b revision 1.

Reason: In accordance with NPPF paragraph 55, to ensure protection of existing trees that make a significant contribution to the amenity of the area and to biodiversity and are fundamental to whether the scheme would be acceptable when assessed against policies within the NPPF.

7 Construction management

Prior to commencement of development a Construction Management Plan shall be approved in writing by the Local Planning Authority. The approved plan shall be adhered to throughout the construction period.

The plan shall include: -

- a) Vehicle parking facilities for site operatives and visitors.
- b) Means of access control, and measures to prevent construction/delivery vehicles parking/blocking the highway.
- c) Details of measures to keep the highway clean - to include wheel washing facilities for the cleaning of vehicles leaving the site, to include location and type.
- d) Dust - A site-specific risk assessment of dust impacts in line with the guidance provided by IAQM (see <http://iaqm.co.uk/guidance/>) and including a package of mitigation measures commensurate with the risk identified in the assessment.
- e) Air Quality - The air quality impacts associated with construction vehicles and non-road mobile machinery (NRMM) and the proposed mitigation measures, commensurate with the identified risk.
- f) Noise - Details on types of machinery to be used, noise mitigation, any monitoring and compliance with relevant standards. Hours of working.
- g) Vibration - Details on any activities that may result in excessive vibration, e.g. piling, and details of monitoring and mitigation to be implemented.
- h) Lighting - Details on artificial lighting and measures to minimise impact, such as restrictions in hours of operation, and the location and angling of lighting.
- i) Complaints procedure - The procedure should detail how a contact number will be advertised to the public, investigation procedure when a complaint is received, any monitoring to be carried out, and what will happen in the event that the complaint is not resolved. Written records of any complaints received and actions taken shall be kept and details forwarded to the Local Authority

every month during construction works by email to the following addresses - public.protection@york.gov.uk and planning.enforcement@york.gov.uk

Reason: To ensure before development commences that construction methods will safeguard the amenities of neighbouring properties in accordance with Policy ENV2 of the City of York Publication Draft Local Plan 2018.

8 Construction working hours

During construction works the hours of construction, including loading or unloading on the site shall be confined to 8:00 to 18:00 Monday to Friday, 9:00 to 13:00 Saturday and no working on Sundays or public holidays.

Reason: To protect the amenities of adjacent residents.

9 Public sewers

Prior to demolition or groundworks within 5 metres of the centreline of the public sewer which crosses the site, measures to protect the sewer shall be submitted to the Local Planning Authority, approved in writing and the development hereby approved shall take place in accordance with the approved measures thereafter. The protection measures shall include details in respect of demolition and site enabling works, and the construction and hard and soft landscaping.

Reason: In the interests of good design and flood risk, to protect Yorkshire Water infrastructure.

10 Archaeology - Watching Brief

a) No archaeological evaluation or development shall take place until a written scheme of investigation (WSI) for evaluation and watching brief has been submitted to and approved by the local planning authority in writing. The WSI shall conform to standards set by LPA and the Chartered Institute for Archaeologists.

b) The evaluation and post investigation assessment shall be completed in accordance with the programme set out in the Written Scheme of Investigation approved under part (a) and the provision made for analysis, publication and dissemination of results and archive deposition will be secured. (This part of the condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the WSI).

c) A copy of a report on the evaluation and an assessment of the impact of the proposed development on any of the archaeological remains identified in the evaluation shall be deposited with City of York Historic Environment Record to allow public dissemination of results within 6 weeks of completion or such other period as

may be agreed in writing with the Local Planning Authority.

d) Where archaeological features and deposits are identified proposals for the preservation in-situ, or for the investigation, recording and recovery of archaeological remains and the publishing of findings shall be submitted as an amendment to the original WSI. (There shall be presumption in favour of preservation in-situ wherever feasible.

Reason: In accordance with Section 16 of NPPF. The site lies within an area of archaeological interest. An investigation is required to identify the presence and significance of archaeological features and deposits and ensure that archaeological features and deposits are either recorded or, if of national importance, preserved in-situ.

11 Archaeology - Building recording

Prior to demolition of buildings 1-12 Ordnance Lane a written scheme of investigation (WSI) for a building recording shall be submitted to and approved by the local planning authority in writing. No demolition of the relevant buildings shall take place until the photographic recording as proposed in the WSI has been carried out. A copy of the recording shall be deposited with City of York Historic Environment Record and digital archive images with ADS to allow public dissemination of results within 3 months of completion or such other period as may be agreed in writing with the Local Planning Authority.

Reason: In accordance with Section 16 of NPPF and Policy D7 of the Publication Draft York Local Plan 2018 as the buildings on this site are of historic and social interest and must be recorded prior to demolition.

12 Nesting birds

No tree maintenance, vegetation clearance, demolition or renovation works to the buildings on site shall take place between 1st March and 31st August inclusive, unless a competent ecologist has undertaken a careful, detailed check of suitable habitat for active birds' nests immediately before the works and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation shall be submitted to the local planning authority in advance of works.

Reason: To ensure that breeding birds are protected from harm during construction. All British birds, their nests and eggs (with certain limited exceptions) are protected by Section 1 of the Wildlife and Countryside Act 1981, as amended.

13 LC1 Land contamination - Site investigation

- 14 LC2 Land contamination - remediation scheme
- 15 LC3 Land contamination - remedial works
- 16 LC4 Land contamination - unexpected contamination
- 17 Drainage

Surface water run-off from the site shall be no more than 75.2 litres per second. Site specific details of the final drainage strategy shall be submitted to and approved in writing by the Local Planning Authority prior to commencement of construction of the development hereby permitted. The development shall be carried out in accordance with the approved scheme.

Site specific details shall include the following -

- The means by which the surface water discharge rate shall be restricted to a maximum rate of 75.2 litres per second. To include the means by which the on-site surface water attenuation shall accommodate up to the 1 in 100 year event with a 30% climate change allowance shall be achieved.
- Storage volume calculations, using computer modelling, which must accommodate a 1:30 year storm with no surface flooding, along with no internal flooding of buildings or surface run-off from the site in a 1:100 year storm. Proposed areas within the model must also include an additional 30% allowance for climate change. The modelling must use a range of storm durations, with both summer and winter profiles, to find the worst-case volume required.
- Existing and proposed ground levels.
- Future management and maintenance of the proposed drainage scheme.

Reason: In the interests of preventing increased flood risk, as required under NPPF section 15, policy ENV5 of the Publication Draft Local Plan 2018 and the City of York Council Sustainable Drainage Systems Guidance for Developers.

- 18 Biodiversity net gain

Biodiversity enhancements shall be provided in accordance with the recommendations set out in section 5.4 of the Landscape Statement dated 12.11.2021) provided by Urbed Ltd. Details of the scheme shall be submitted to and approved in writing by the Local Planning Authority prior to construction of the buildings hereby permitted and the enhancements shall be installed in accordance with the approved details prior to first occupation of the relevant building.

The development shall operate / be occupied in accordance with the measures set out in the Landscape Management and Maintenance report by Urbed dated 3.11.2021.

Reason: To enhance the biodiversity and wildlife interest of the area, in accordance with Paragraph 174 d) of the NPPF.

19 Noise insulation to dwellings

The building envelope of all dwellings shall be constructed to achieve internal noise levels in habitable rooms as follows -

Daytime (07:00-23:00 hrs)

- No greater than 35 dB LAeq (16 hour)

Night (23:00-07:00 hours)

- 30 dB LAeq (8 hour)

- LAFMax level should not exceed 45dB(A) on more than 10 occasions in any night time period in bedrooms and should not regularly exceed 55dB(A).

These noise levels shall be observed with windows open in the habitable rooms or if necessary windows closed and other means of ventilation provided.

Prior to construction of the relevant residential building, a detailed scheme of noise insulation measures for protecting the approved dwellings from externally generated noise shall be submitted to and approved in writing by the Local Planning Authority. The building shall be constructed in accordance with the approved details.

Reason: To protect the amenity of people living in the new property from externally generated noise, in accordance with the NPPF paragraphs 130 and 185.

20 Materials

The external materials to be used shall be as annotated on the approved drawings. Prior to construction of the building envelope of the dwellings hereby approved details of the following items shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details -

- a) Manufacturer's details and colour finish of render, vertical hung clay tiles and metalwork.
- b) Sample panels of the brickwork to be used (which shall be erected on the site and shall illustrate the colour, texture and bonding of brickwork and the mortar treatment to be used). This panel shall be retained until a minimum of 2 square metres of wall of the approved development has been completed in accordance with the approved sample.

Reason: In the interests of visual amenity and local distinctiveness, in accordance

with paragraph 127 of the NPPF.

21 Large scale details

Large scale typical details of the following items shall be submitted to and approved in writing by the Local Planning Authority prior to construction of the building envelope of the dwellings hereby approved. The development shall be carried out in accordance with the approved details.

blocks 1-5

doors and windows in context

balconies and balustrades in context

block 6

doors and windows in context

block 7

metal clad walkway and storage

new windows to west elevation

block 8

balconies and balustrades in context

blocks 9 and 10

doors and windows in context

boundary walls and railings

Reason: In the interests of good design, in accordance with NPPF paragraph 130.

22 Secure by Design

The development shall not be occupied until there has been submitted to, and approved in writing by the Local Planning Authority details of secure by design measures. The approved measures shall be instated prior to first occupation of the relevant buildings. The measures shall include details of access control (to private/semi-private spaces and cycle storage).

Reason: In accordance with NPPF paragraph 130f to ensure crime and disorder, and the fear of crime do not undermine the quality of life or community cohesion and resilience.

23 Landscaping / public realm

The development shall not be occupied until there has been submitted to, and approved in writing by the Local Planning Authority, a detailed hard and soft

landscaping scheme (following the principles shown on submitted urbed landscape and tree planting drawings (although it is noted planting between blocks 3 and 4 may be compromised by presence of existing drainage)). The scheme shall illustrate the number, species, height and position of trees and shrubs, tree planting details and details of play equipment, street furniture and surfacing.

Tree planting details shall include: means of support, protection (including prevention of strimmer damage), irrigation; soil volumes and structural soil cell systems where applicable, and the corresponding surfacing detail, locations of underground utilities; maintenance regime and responsibilities. Where trees are to be located within, or adjacent to, hard-standing, the surface area of soil cell systems, soil volumes, and tree species, and any utilities, shall also be shown on a tree planting plan.

The approved landscaping scheme shall be installed in accordance with the approved plans and details prior to first occupation of the development hereby permitted, unless an alternative phasing scheme has been approved in writing by the Local Planning Authority, which shall subsequently be implemented in accordance with the approved details.

Any trees or plants within the public realm which die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of a similar size and species, unless alternatives are agreed in writing by the Local Planning Authority.

Reason: In the interests of good design, in accordance with paragraph 130 of the NPPF.

24 Resident's Parking

Prior to first occupation of the development hereby permitted details of a scheme to provide residents parking, both within the site and in the local area, shall be submitted to and approved in writing by the Local Planning Authority. The approved schemes shall be fully implemented thereafter.

Reason: To reasonably control on-street car parking locally, in the interests of highway network management and in accordance with NPPF paragraphs 110d) and 111.

25 Electric Vehicles

Prior to occupation of 25% of the dwellings hereby permitted the electric vehicle charging facilities (and vehicles) detailed below shall be provided on-site and made available for public use at all times thereafter -

- Electric cargo bike storage, four charging points and hire bikes as detailed in section 4.5 of the Transport Statement V1.0 (by Civic Engineers).
- Two car parking spaces with rapid and fast charging and a fast charging facility serving the car club bay. The parking spaces, as shown on the approved site plan, shall be exclusively for use by electric or car club vehicles.

In addition to the above an electric vehicle charging facility for at least one of the parking spaces to the north of blocks 7 and 8 shall be provided prior to first occupation of blocks 7 and 8. The details of the provision shall be approved by the Local Planning Authority in advance of installation.

Reason: To encourage the use of electric vehicles and reduce emissions, in accordance with paragraphs 110 and 186 the NPPF and policy ENV1 of the Publication Draft Local Plan 2018.

26 Travel Plan

The development hereby permitted shall be occupied in accordance with the Civic Travel Plan V1.0.

Following the annual surveys and reports should the travel plan targets not be met details of further actions and measures to achieve the plan targets shall be submitted (by the travel plan co-ordinator) to the Local Planning Authority for approval and subsequently implemented in accordance with the approved details.

Reason: In order to promote sustainable travel, in accordance with section 9 of the NPPF and Publication Draft Local Plan 2018 policy T7.

27 Cycle and bin storage

The cycle and bin storage shall be provided in accordance with the approved plans prior to first occupation of the relevant buildings. The facilities shall be unobstructed and retained for such use at all times.

Reason: To promote sustainable transport and in the interests of good design in accordance with section 9 of the NPPF.

28 Commercial units – times of operation

The commercial units hereby permitted shall only be open to customers between the hours of 07:00 and 23:00 each day of the week.

All deliveries and collections to the commercial units shall only occur during the following times -

Monday to Friday 08:00 to 18:00 hours

Saturday 09:00 to 13:00 hours
Not at all on Sundays and Bank Holidays

Reason: To protect the amenity of occupants of the nearby properties from noise, in accordance with the NPPF paragraphs 130 and 185.

29 Commercial unit – any plant and machinery

The combined rating level of any building service noise associated with plant or equipment within the commercial and community units shall not exceed the representative LA90 1 hour during the hours of 07:00 to 23:00 or representative LA90 15 minutes during the hours of 23:00 to 07:00, at 1 metre from the nearest noise sensitive facades when assessed in accordance with BS4142: 2014, inclusive of any acoustic feature corrections associated with tonal, impulsive, distinctive or intermittent characteristics.

Reason: To protect the amenity of surrounding residential in accordance with NPPF paragraphs 130 and 185.

30 Sustainable construction

The dwellings hereby permitted shall achieve a reduction in carbon emissions of at least 28% compared to the target emission rate as required under Part L of the Building Regulations 2013.

Reason: To fulfil the environmental objectives of the NPPF and support the transition to a low carbon future, and in accordance with policies CC1 and CC2 of the Publication Draft Local Plan 2018.

31 Overlooking

The roof terrace screens for house type F, as shown on drawing (00) 126 P2, shall be installed prior to first occupation of the relevant dwellings and retained as such at all times.

Reason: To prevent overlooking of surrounding homes in accordance with NPPF paragraph 130.

8.0 INFORMATIVES:

1. STATEMENT OF THE COUNCIL'S POSITIVE AND PROACTIVE APPROACH
In considering the application, the Local Planning Authority has implemented the

requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application. The Local Planning Authority took the following steps in order to achieve a positive outcome: sought revised plans and negotiation to address issues in respect of amenity, highways and drainage and through the use of planning conditions.

Contact details:

Case Officer: Jonathan Kenyon

Tel No: 01904 551323

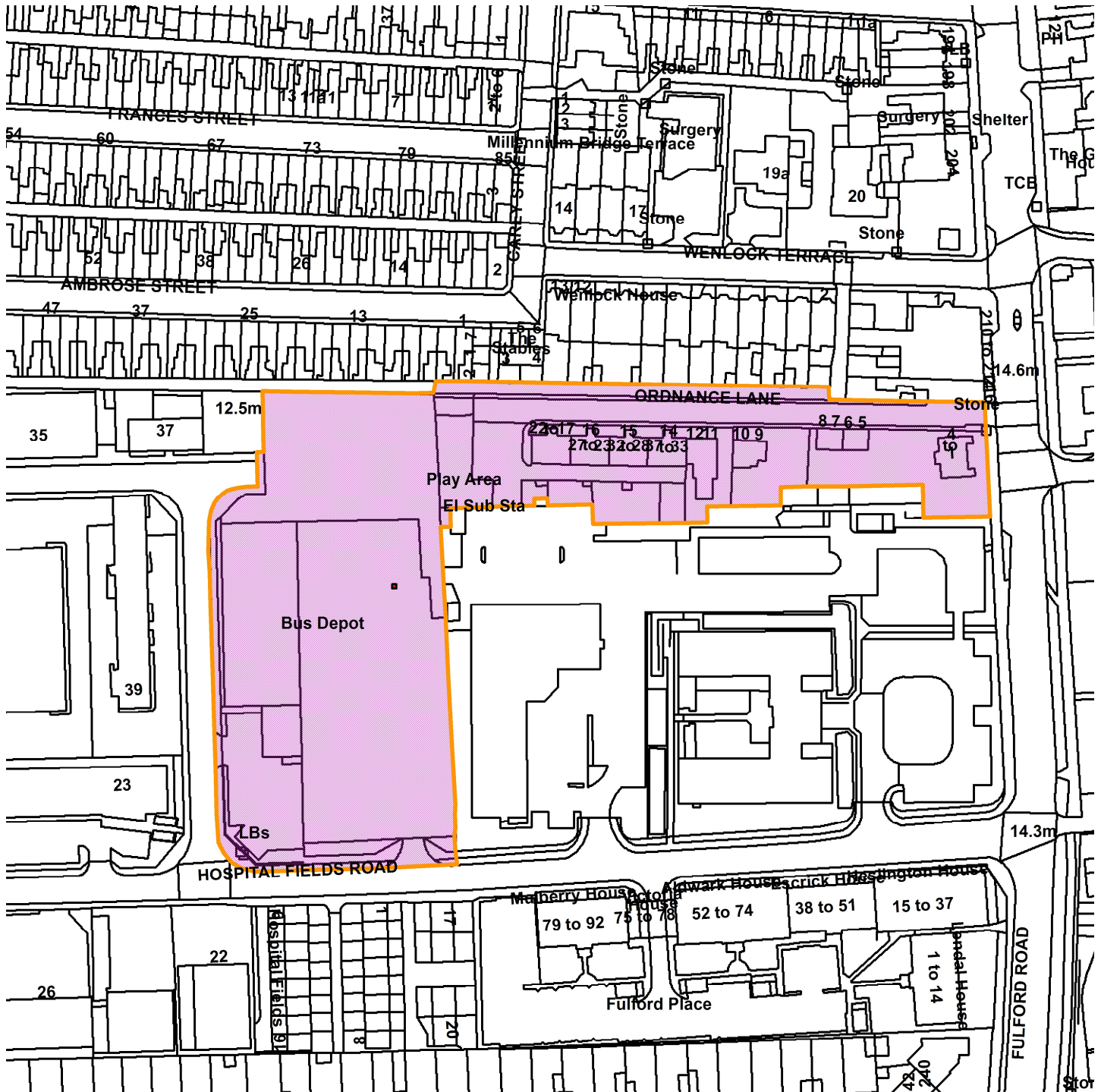
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21/02573/FULM

Development Site, Hospital Fields Road And Ordnance Lane, York



GIS by ESRI (UK)



Scale : 1:1632

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Organisation	City of York Council
Department	Directorate of Place
Comments	Site location plan
Date	26 May 2022
SLA Number	

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Planning Committee A

To be held on Thursday 9th June 2022 at 4:30pm

21/02573/FULM – Development Site Hospital Fields Road and Ordnance Lane, York

Demolition of existing buildings, alterations to 'The Married Quarters Building', and erection of new buildings to provide no.85 residential dwellings (Use Class C3), 457sqm of commercial / business floorspace (Use Class E) and 152sqm of community floorspace (Use Class F1/F2) with associated ancillary development

Fulford Road boundary wall & frontage building (1)



Fulford Road boundary wall & frontage building (2)



Page 54

Ordnance Lane buildings proposed for demolition



Page 55

Married Quarters (2-storey extension to be demolished)



Page 56



Views into the site and of Ambrose Street from the South



Warehouse on south side of the site



Offices/Nursery/Gym within buildings to south of the site



Wenlock Terrace



Alleyway to West of site



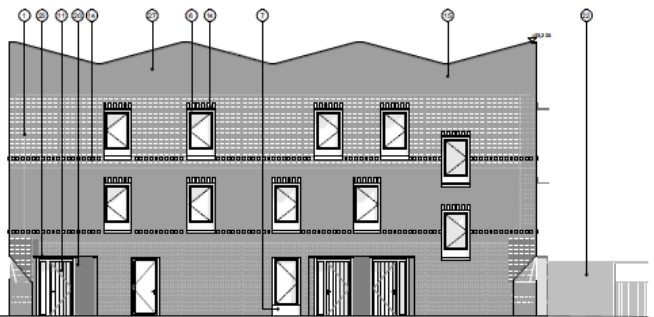
Blocks 1-4 Typical Elevations



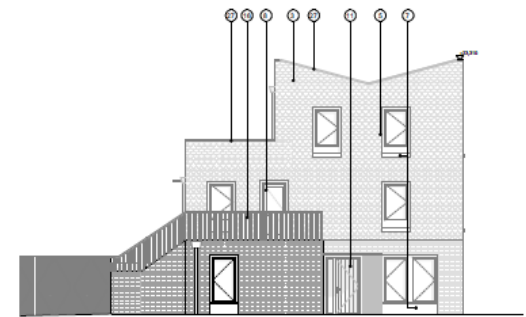
Blk 1 - South Elevation



Blk 2 - South Elevation



Blk 3 - West Elevation



- MATERIAL KEY**
- Red Mill Brick with Flush and Brushed Pointing
 - Flintwork
 - Lightweight Hung Clay Tile
 - Red Mill Brick Finish
 - Aluminium window
 - Slater Shingles
 - Pre-cast concrete Lintel/Window Cill
 - Glazed Composite Door
 - Glazed Composite Door with sidelight
 - Solid Composite Door
 - Solid Composite Door with Sidelight
 - Glazed and Steel Gutters/Coverings
 - Black Pan Tiles
 - Decorative Brickwork
 - Integrated Flat Roof/Flat Panels
 - Timber Balustrade
 - Steel Frame Balcony
 - Powder Coated Mesh Garden Fencing
 - Timber Garden Gate
 - Powder Coated Steel Electrical & Postal Box
 - Powder Coated Expanded Aluminium Mesh and Brick Slim Store
 - Powder Coated Expanded Aluminium Mesh and Brick Slim Store
 - Powder Coated Expanded Aluminium Mesh and Brick Front Porch
 - Casting Balustrade
 - Brick Plinth
 - Metal Structure
 - Zinc Cladding
 - Timber Sash
 - Timber Door
 - Timber Cladding
 - Crest & Glazed Timber Sash Window
 - Recessed Metal Door
 - Recessed Brickwork Panel
 - Steel Roof Cladding
 - Powder Coated Metal Sheet
 - Pre-finished Metal Door
 - Glazed and Steel Door with Aislant
 - Timber Water But

Scale 1:100

Revised	Issued	Issue Title
01	01/11/2021	Planning

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HEALTH, SAFETY AND ENVIRONMENTAL INFORMATION BOX
 Note: This drawing is to be read in conjunction with the risk register specified for inclusion in the health and safety plan. The hazard notes are in addition to normal hazards and shall be read by a competent contractor when dealing with the type of works detailed on the drawing.

**NOT FOR CONSTRUCTION
 DO NOT SCALE FROM THIS DRAWING**

All measurements given are indicative, on site dimensions must be determined prior to the purchase and installation of any components. All dimensions shall be taken in accordance with the industry standard and shall be taken in accordance with the drawing.

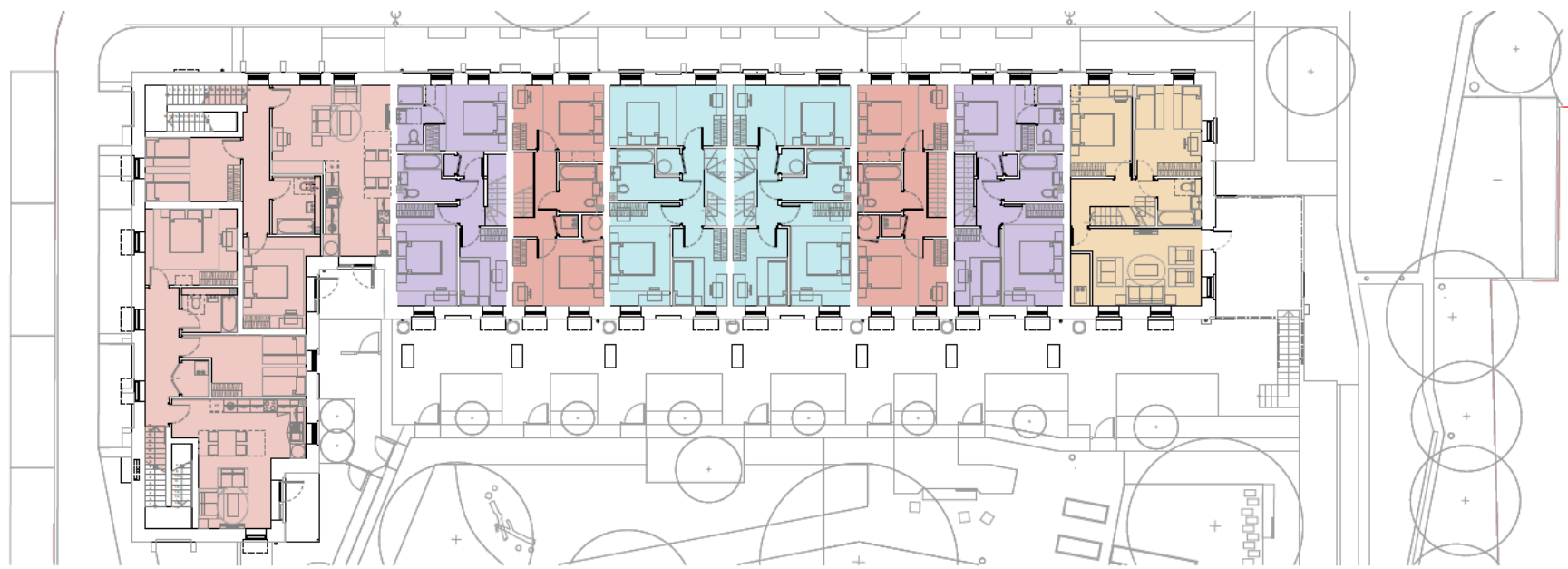
DRAWING TITLE: 288C | Ordinance Lane

Block 4 Elevations

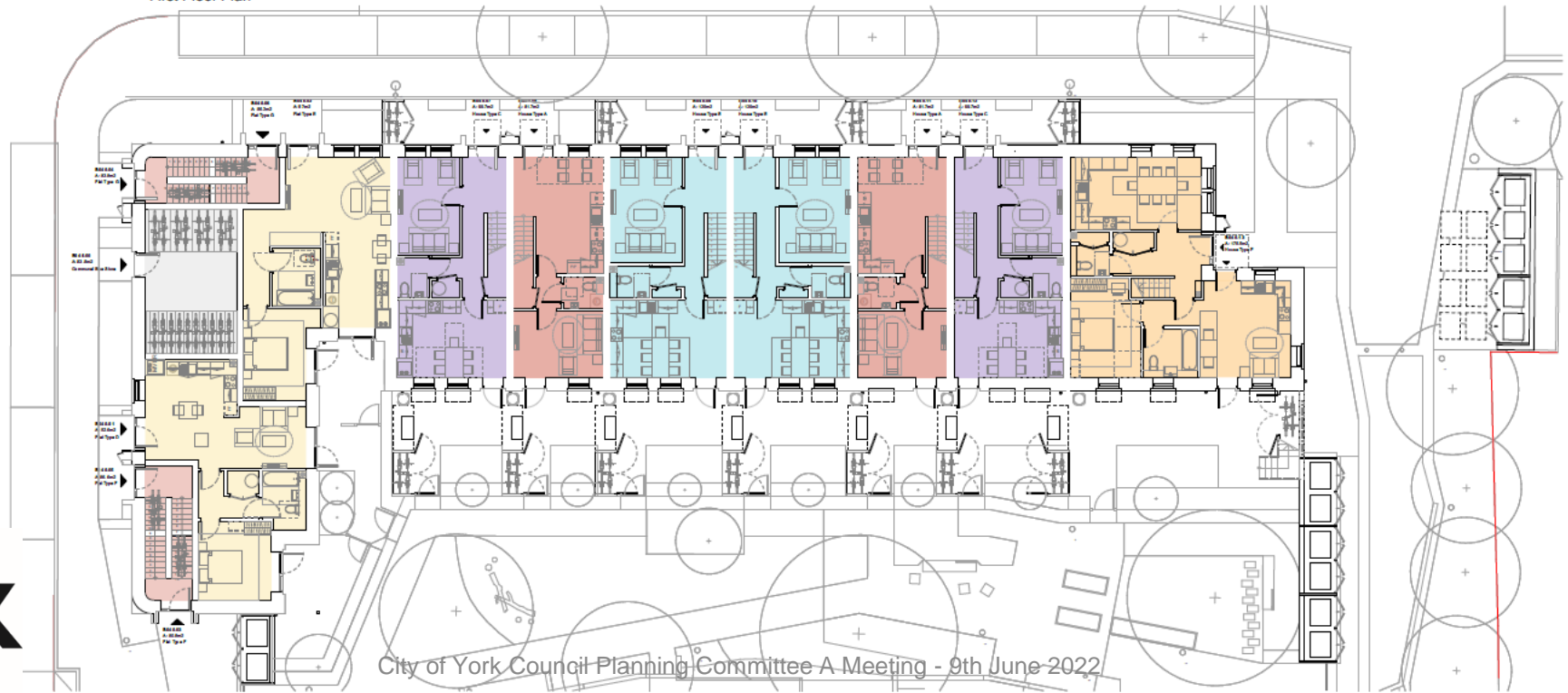
PROJECT: Ordinance Lane	DATE: 01/11/2021
CLIENT: NEP - City of York Council	SCALE: 1:100 @ A1
DRAWING STATUS: Rev	Revision
Project No: 288C	Drawing No: (00) 204
Client No: C	Page No: P1



Blocks 1-4 Typical Floor Plan Ground and First Floor



First Floor Plan



City of York Council Planning Committee A Meeting - 9th June 2022

KEY

- Commercial Unit
- Common
- 1st/2p (1 storey)
- 2nd/3p (1 storey)
- 2nd/4p (1 or 2 storey)
- 2nd/5p (2 storey)
- 4th/7p (2 storey)
- 5th/10p (2 storey)
- Site Boundary
- Roof Protection Area
- Tree Crown



Scale: 1:100

Revised	Issued	Author
01	03/12/22	Planning

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HEALTH, SAFETY AND ENVIRONMENTAL INFORMATION BOX
 Note: This drawing is to be used in conjunction with the risk register produced for inclusion in the health and safety plan. The hazard notes are in addition to normal health and safety best practice and should be read in conjunction with the type of work detailed on the drawing.

**NOT FOR CONSTRUCTION
 DO NOT SCALE FROM THIS DRAWING**

All dimensions given are in millimetres, unless otherwise stated. Dimensions should be checked prior to the start of construction of any component. All dimensions should be rounded up to the nearest millimetre and those indicated on site to be brought to attention.

DRAWING TITLE: 288C | Entrance Lane

Block 4 GA Plans GF & 1F

PROJECT: Entrance Lane

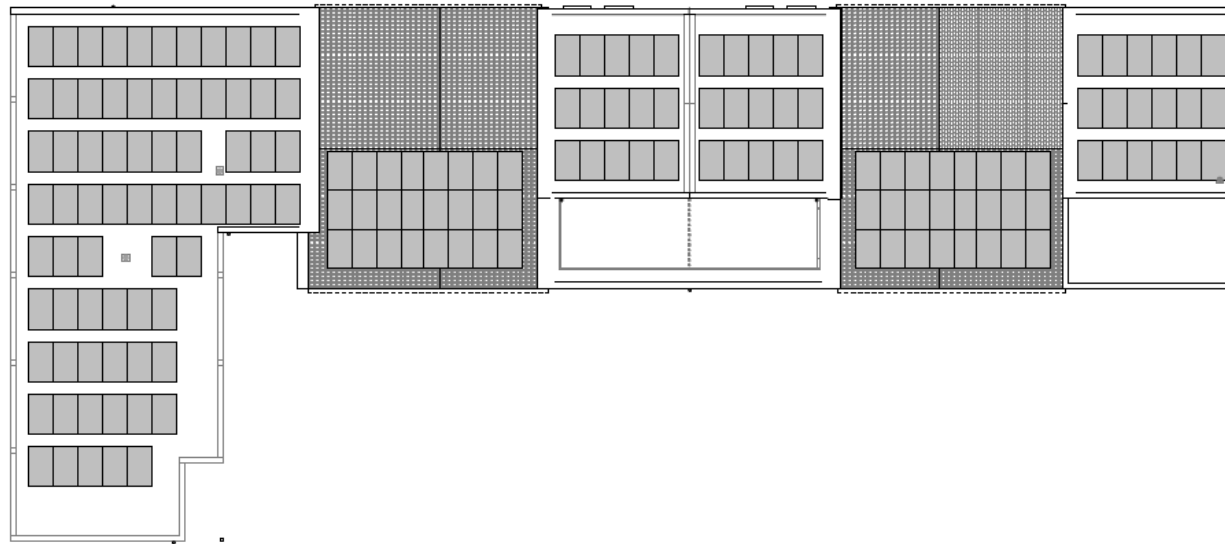
CLIENT: HEP - City of York Council DATE: 04/12/21

DRAWING STATUS: PLANNING SCALE: 1:100 @ A1

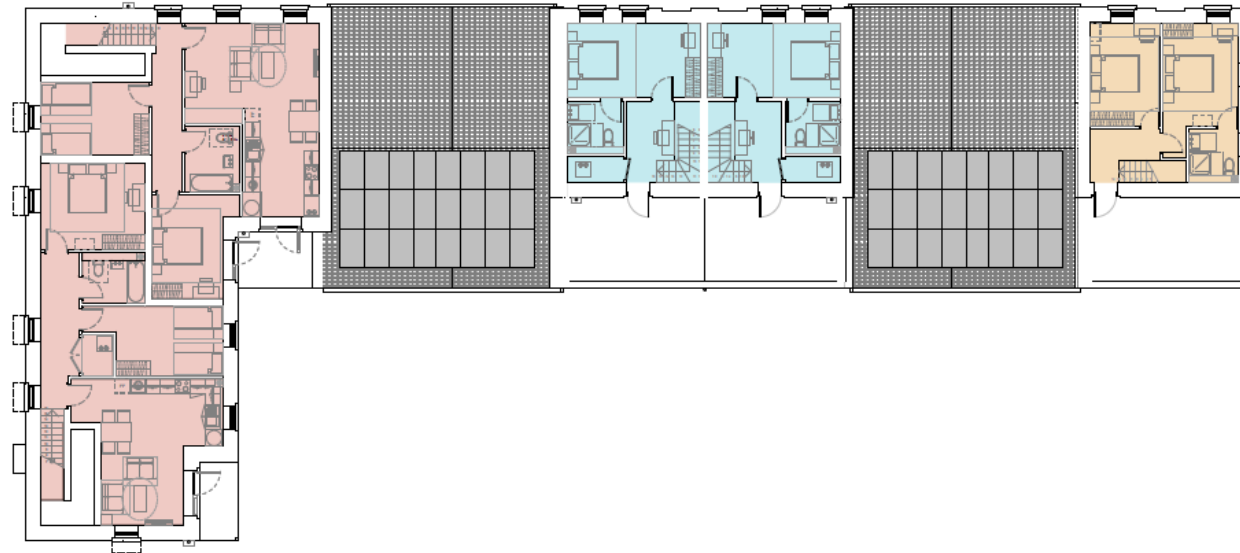
Drawn: 13 Drawing: Revision: P1
 288C C (00) 107



Blocks 1-4 Typical Floor Plan Second Floor and Roof Plan



Roof Plan



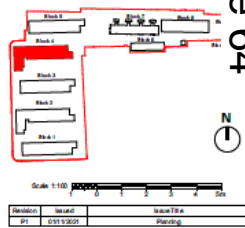
Second Floor Plan

City of York Council Planning Committee A Meeting - 9th June 2022



KEY

- Connected Unit
- Common
- 1st flp (1 way)
- 2nd flp (1 way)
- 2nd flp (1 or 2 way)
- 3rd flp (2 way)
- 4th flp (3 way)
- 5th flp (3 way)
- Site Boundary
- Roof Protection Area
- Tree Crown



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HEALTH, SAFETY AND ENVIRONMENTAL INFORMATION BOX
 Notes: This drawing is to be used in conjunction with the risk register provided for inclusion in the health and safety plan. The hazard analysis in addition to formal methods and risk levels by a competent contractor when dealing with the type of work detailed on the drawing.

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All measurements given are indicative, on site dimensions must be determined prior to the project. Measurements are approximate. All dimensions are given unless otherwise stated. All dimensions are to be brought to each level attention.

DRAWING TITLE: 288C | Ordnance Lane

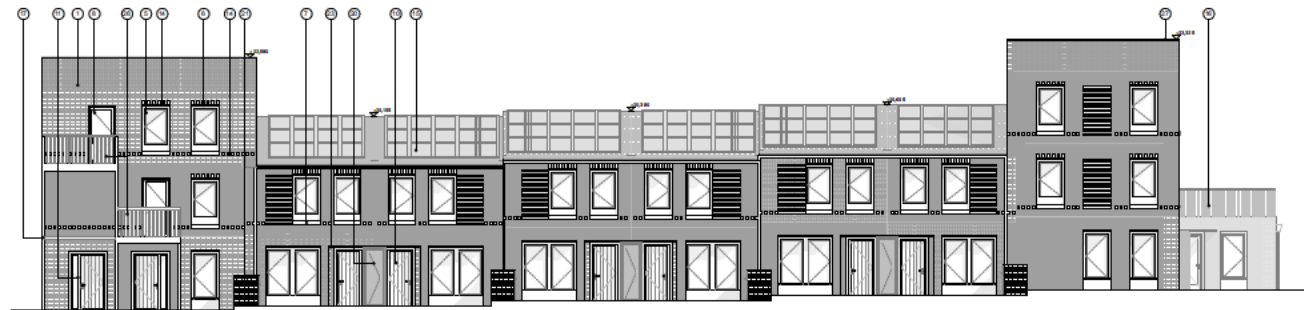
Block 4 GA Plans 2F & Roof Plan

PROJECT	Ordnance Lane
CLIENT	HEP - City of York Council
DRAWING STATUS	PLANNING
DATE	06/11/2021
SCALE	1:100 @A1
Revision	1 of 1
288C C	(00) 108 P1

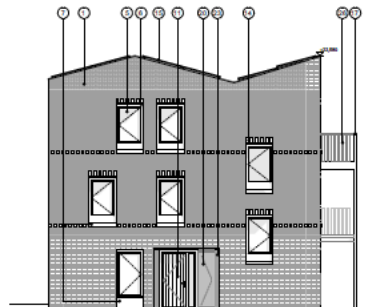
Block 5 Proposed Elevations



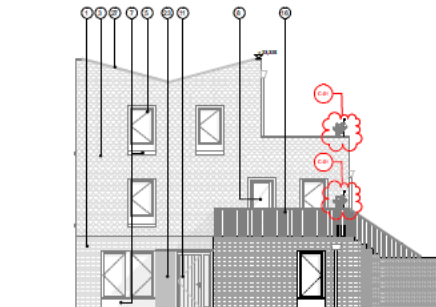
28.6.1 East Elevation



28.6.2 South Elevation



28.6.3 West Elevation



28.6.4 East Elevation



Revision	Change ID	Change Name
01		
02	0.01	West Window Screen Added


MATERIAL KEY

- Red M20 Brick with Flush and Brushed Pointing
- Render
- Lightweight Hung Clay Tiles
- Red M20 Brick Pitch
- Aluminium Window
- Solar Shielding
- Pre-cast concrete Linear Window Cill
- Glazed Composite Door
- Glazed Composite Door with side light
- Solid Composite Door
- Solid Composite Door with side light
- Galvanneal Steel Composite Door/Window
- Black Paintwork
- Decorative Blinds
- Integrated Photovoltaic Panels
- Timber Balustrade
- Steel Frame Balustrade
- Powder Coated Metal Mesh Garden Fencing
- Timber Garden Gate
- Reinforced Metal Sheet Cladded R Panel Box
- Powder Coated Expanded Aluminium Mesh and Slats
- Powder Coated Expanded Aluminium Mesh and Slats
- Render Coated Expanded Aluminium Mesh and Slats
- Cutting Blinds
- Blind Pitch
- Metal Balustrade
- Zinc Cladding
- Timber Gate
- Timber Door
- Timber Cladding
- Double Glazed Timber Sash Window
- Secure Metal Mesh Door
- Recessed Sliding Panel
- Slate Roof Cladding
- Render Coated Metal Mesh
- Reinforced Metal Door
- Glazed Aluminium Door with Aluvent
- Timber Mesh Sun
- Render Coated Expanded Aluminium Mesh Screen

Scale 1:100

Revision	Issue	Submittal
01	001/2022	Permitting
02	002/2022	Permitting - Revise in

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 Note: This drawing is to be used in conjunction with the full range of documents provided for information in the health and safety plan. The hazard notes are in addition to normal health and safety notes to be provided by a competent contractor when dealing with the type of work detailed on the drawing.

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 All measurements given are indicative and the manufacturer's details should refer to the purchaser's literature of any components. All dimensions are between finished surfaces unless otherwise indicated and are measured on site to be brought to work as shown.

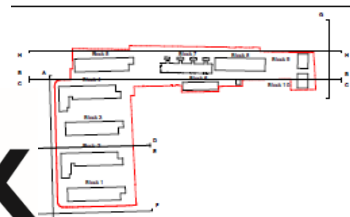
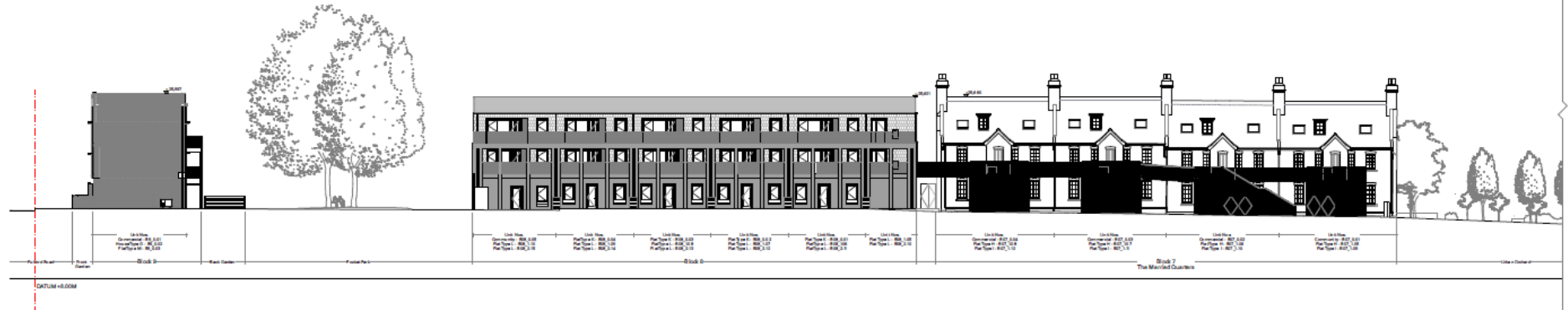
DRAWING TITLE: 288C | Ordinance Laws

Block 5 Elevations

PROJECT	DATE
Ordinance Laws	09/06/2022
CLIENT	DATE
HEP - City of York Council	09/06/2022
DRAWING STATUS	SCALE
PLANNING	1:100 @ A1
FIGURE	SECTION
288C C	001 205



Blocks 7 (Married Quarters) and 8 North Facing Elevations



Revision	Number	Issue Date
01	01/23/2021	Planning
02	08/03/2022	Planning - Revision

Revision	Change ID	Change Name
01	C-01	Acad Service Screen Added

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HEALTH, SAFETY AND ENVIRONMENTAL INFORMATION BOX
 ⚠️
 Note: This drawing is to be read in conjunction with the Risk Register produced for inclusion in the health and safety plan. The hazard score will be adjusted to normal hazards and risks raised by a competent contractor when working with the type of materials detailed on the drawing.

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All measurements given are inclusive, in the dimensions shall be determined prior to the commencement of any components. A discrepancy between the above dimensions, plans and their reproduction shall be brought to the contractor's attention.

DRAWING TITLE: 288C | Ordinance Lane

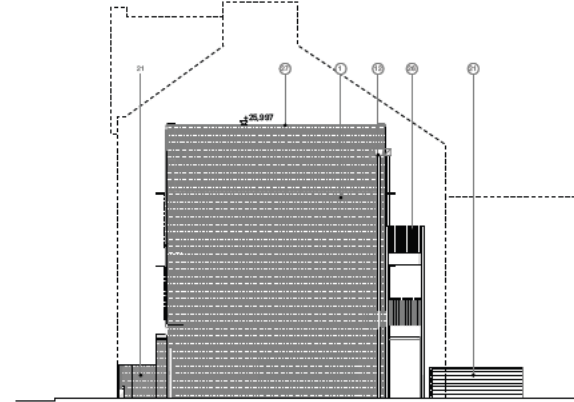
Site Elevations H

PROJECT:	DATE:
Ordinance Lane	09/03/2022
CLIENT:	HEALTH AND SAFETY:
HOP - City of York Council	SCALE:
DRAWING STATUS:	1:200 @ A1
PLANNING	Revision:
Figure No.:	Sheet No.:
288C C	(00) 224
	P2

Block 9 Elevations



Block 9 - East Elevation



Block 9 - West Elevation



Block 9 - South Elevation



Block 9 - North Elevation

MATERIAL KEY

1. Reddish Brick with Flush and Brushed Pointing
2. Render
3. Lightweight Insulating Clay Tile
4. Reddish Brick Plinth
5. Aluminium window
6. Solar Shading
7. Powder Coated Laminated Window CG
8. Glass Composite Door
9. Glass Composite Door
10. Side Composite Door with side gl
11. Side Composite Door with side gl
12. Side Composite Door with side gl
13. Galvanneal Steel Decking/Concrete
14. Black Pan Tiles
15. Decorative Brickwork
16. Integrated Productive Panels
17. Timber Balustrade
18. Steel Frame Balustrade
19. Powder Coated Metal Mesh Garden Fencing
20. Timber Garden Gate
21. Powder Coated Steel Cladded A Frame Box
22. Powder Coated Expanded Aluminium Mesh and Brick tile Stone
23. Powder Coated Expanded Aluminium Mesh and Brick tile Stone
24. Galvanneal Steel Decking/Concrete
25. Black FRP
26. Metal Balustrade
27. Zinc Cladding
28. Timber clade
29. Timber Door
30. Timber Cladding
31. Dark Coloured Timber Sash Window
32. Secure Vertical Mesh Door
33. Resealed Brickwork Panel
34. Stone Roof Cladding
35. Powder Coated Metal Sheet
36. Powder Coated Metal Sheet
37. Galvanneal Steel Decking/Concrete
38. Timber Water But



Revision	Issue	Issue Date
01	01/11/2021	Planning

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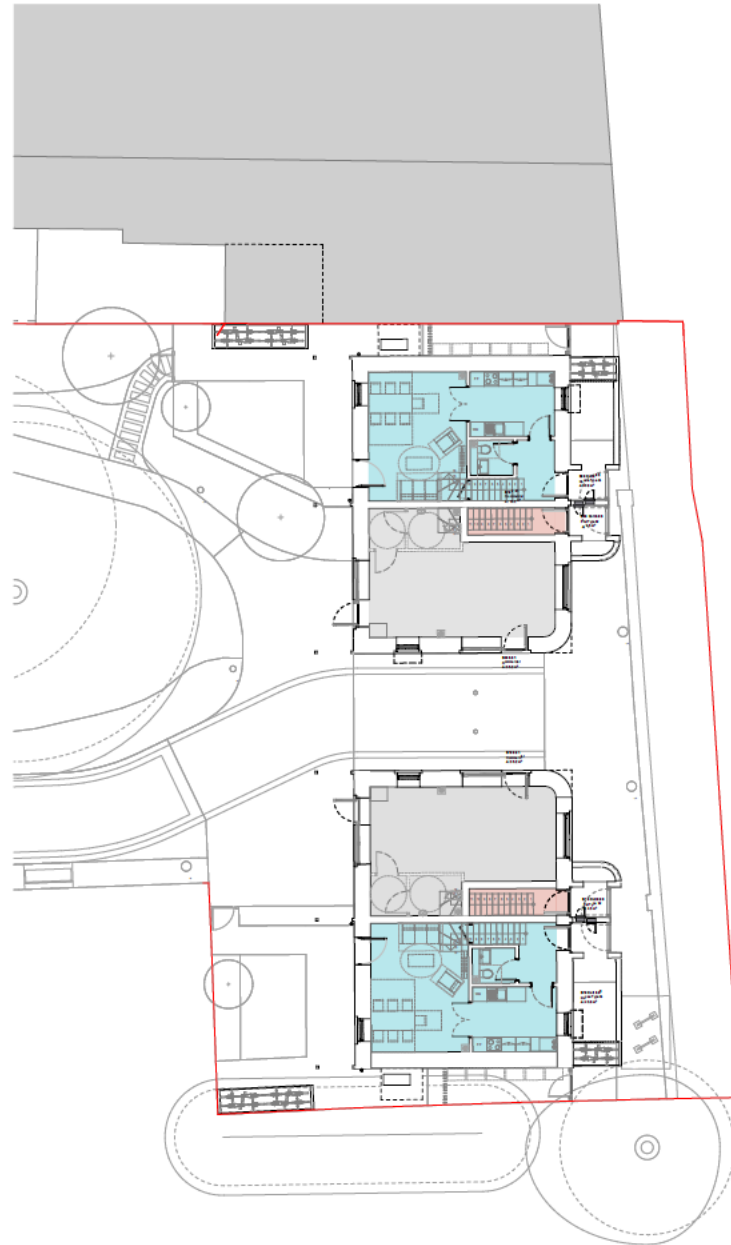
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Block 9 Elevations

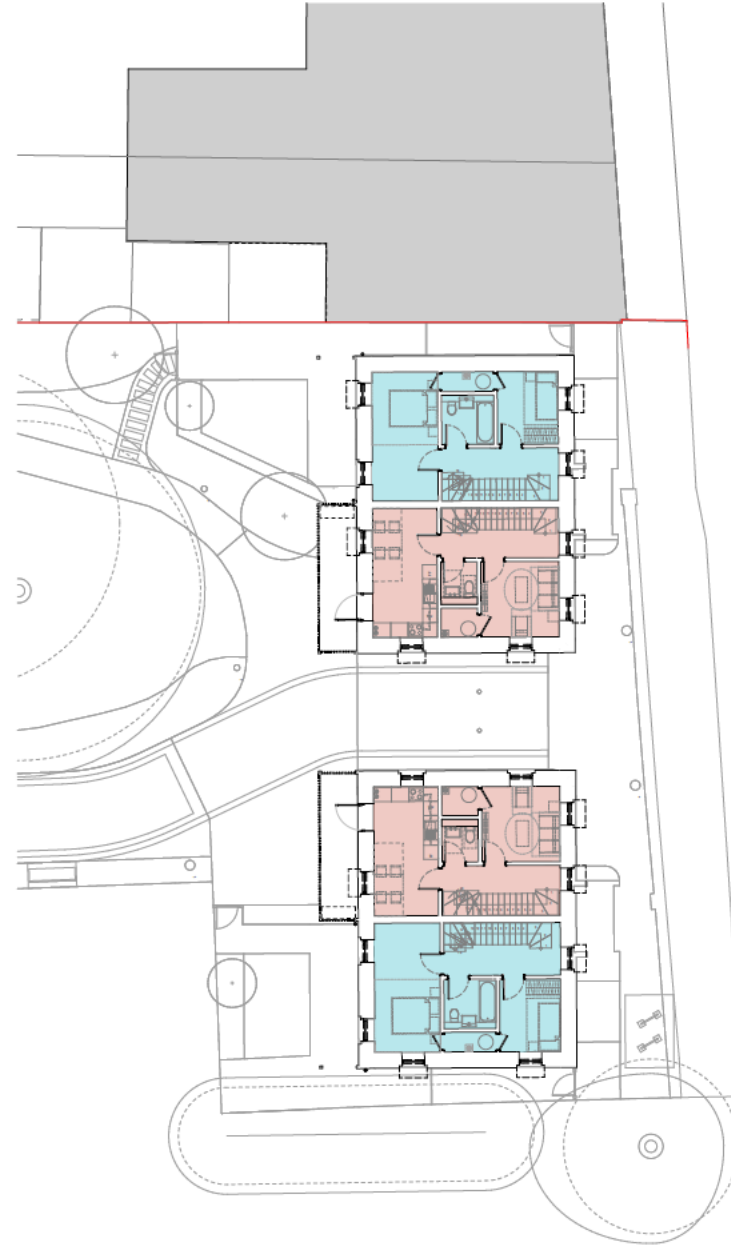
PROJECT: Ordnance Lane
 CLIENT: City of York Council
 DATE: 04/11/2021
 DRAWING STATUS: SCALE E
 PLANNING: 1:100 (pg. A1)
 FIGURE: No. 17
 SHEET: 288C c 17 (00) 209
 DRAWING: 17
 DATE: 04/11/2021
 SCALE: E
 FIGURE: No. 17
 SHEET: 288C c 17 (00) 209
 DRAWING: 17



Blocks 9 and 10 Layout and Floor Plans



Ground Floor Plan



First Floor Plan

City of York Council Planning Committee A Meeting - 9th June 2022



KEY

- Commercial Unit
- Common
- 1st/2p (1 storey)
- 2nd/3p (1 or 2 storey)
- 3rd/4p (2 storey)
- 4th/5p (3 storey)
- 6th/7p (3 storey)
- Site Boundary
- Road Protection Area
- Tree Crown

Scale 1:100

288C | Ordnance Lane

Page 68

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HEALTH, SAFETY AND ENVIRONMENTAL INFORMATION BOX
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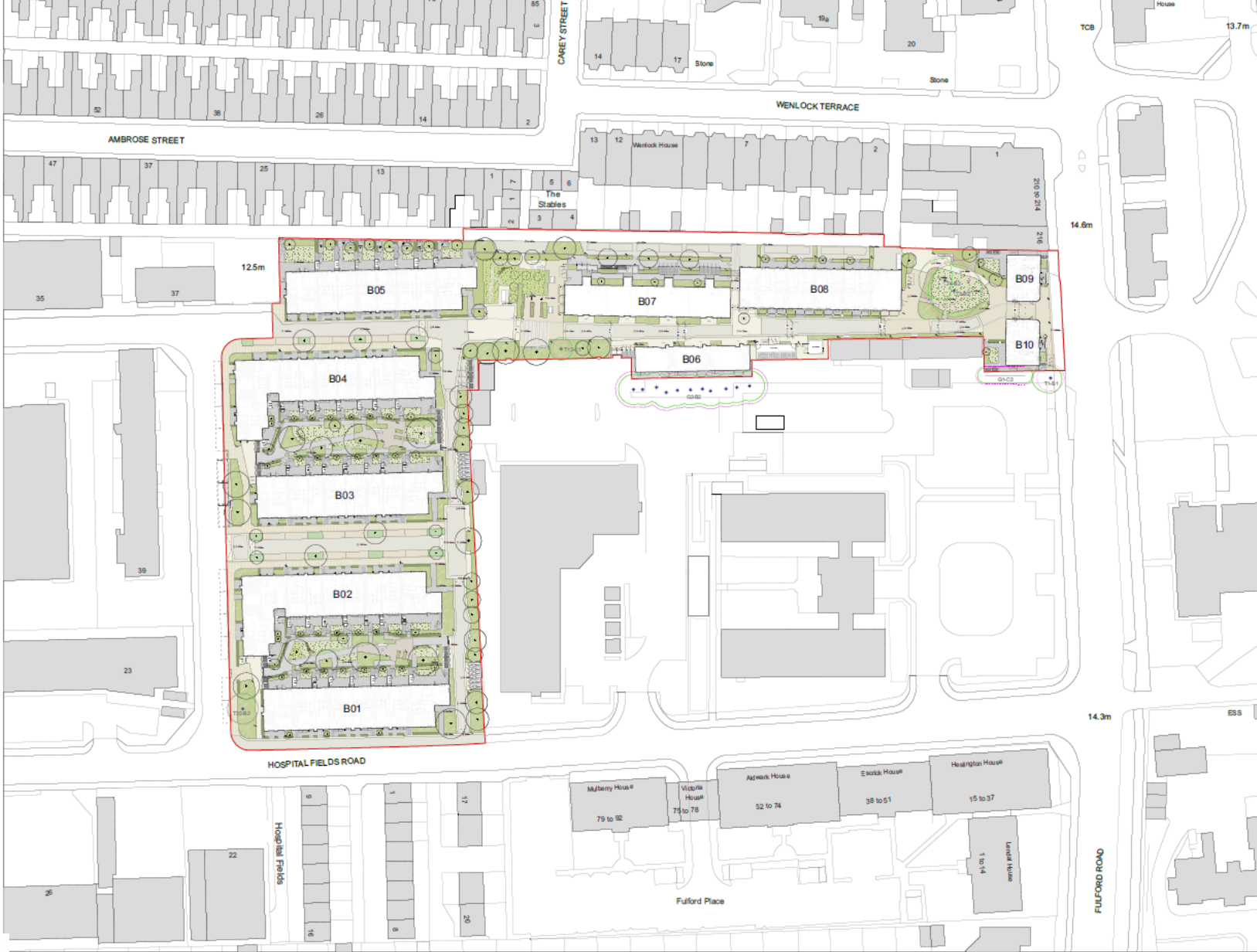
All measurements given are indicative, on site dimensions should always be used prior to the purchase of materials and construction. All discrepancies shall be notified immediately to the client and the contractor at the time of the start of work.

DRAWING TITLE: 288C | Ordnance Lane

Block 9 & 10 GA Plans
GF & 1F

PROJECT	Ordnance Lane	DATE	06/11/2021
CLIENT	HSP - City of York Council	SCALE	1:100 @A1
DRAWING STATUS	PLANNING	Revision	
FIG: 18	18	116	P1

Landscaping Scheme



- Key**
- Development Boundary
 - Red Landscaping (to be used in conjunction with the surrounding area)
 - T1 - Proposed Timber Fencing (to be used in public pedestrian and cycle access areas)
 - T2 - Proposed Concrete Block Paving (concrete pavers) in public pedestrian areas
 - T3 - Proposed Concrete Flag Paving (interlocking flag) in public pedestrian areas
 - T4 - Proposed Concrete Block Paving (into of permeable pavers) in public pedestrian areas
 - T5 - Proposed Concrete Block Paving (into of permeable pavers) in public pedestrian areas
 - T6 - Proposed Asphalt Yorkshire Natural Paving (on permeable sub-base) in public squares
 - T7 - Proposed Concrete Block Paving (concrete pavers) in commercial areas
 - T8 - Proposed Concrete Flag Paving (interlocking flag) in near private garden areas
 - T9 - Proposed Concrete Block Paving (interlocking pavers) in private garden areas
- So & Landscape**
- Category A
 - Category B
 - Category C
 - MS 5037 2012 Root Protection Area
 - Proposed Trees
 - Proposed Hedge Planting
 - Proposed Deciduous Shrub (Paravial Planting)
 - Proposed Mixed Shrub (Paravial Planting)
 - Proposed SuDS Planting
 - Proposed Meadow Planting
 - Proposed An eddy Grass
- Fences and Features**
- Proposed Timber Fencing (1.8m height)
 - Proposed Timber Fencing with Brick Base (2m height)
 - Proposed Timber Fencing in near gardens (with 10 and 15m Fence Panels, c.1.5-1.2m height)
 - Proposed Low Wire Mesh Fencing in near gardens (c.1.2m height)
 - Proposed Tall Wire Fencing (c.1.8m height)
 - Proposed Single Leaf Gate
 - Proposed Bollard
 - Proposed Fixed Timber Wall Top 5 Concrete / Brick Base
 - Proposed Timber Cube Seats
 - Proposed Raised Planting Beds
 - Proposed Water Run in Commercial
 - Proposed Feature Storage Churn
 - Proposed Paving Slab Slipping St
 - Proposed Large Log
 - Proposed Commercial Bin Storage
 - Proposed Commercial Bin Storage
 - Proposed Cycle Stand (P&G)
 - Proposed Cycle Storage (Private)
 - Proposed Air Source Heat Pump (ASHP)
 - Proposed Wheelcut
 - Proposed Landscaped Supplied Water Tap (for commercial use only)
- Lighting Features**
- Proposed Robust Bollard Lighting
 - Proposed Pole Top Light on 5m Column
 - Proposed Street Light on 6m Column



City of York Council Planning Committee A Meeting - 9th June 2022

Project No:	31	Date:	04-12-2020	CD:	CD
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urberd

1128-URBED-Z0-DR-L-Landscape_Plan

Project Title: **Landscaping Scheme**

Project No: **Ordnance Lane**

Client: **HDP - City of York Council**

Project Number: **1128**

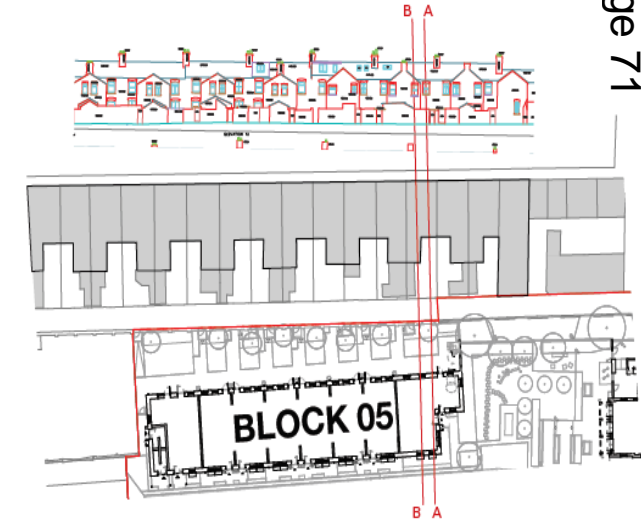
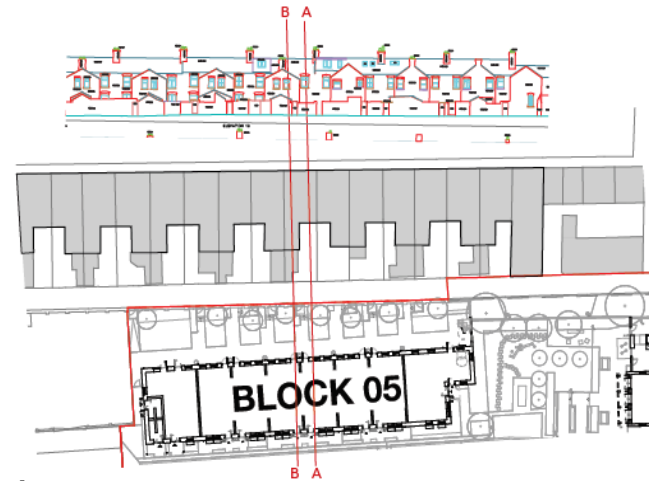
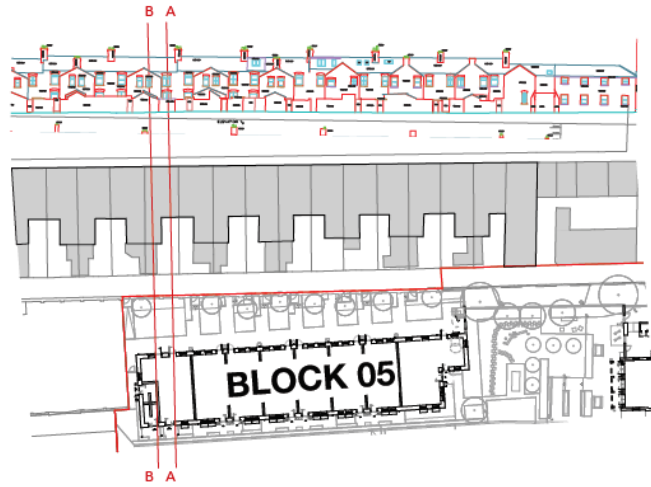
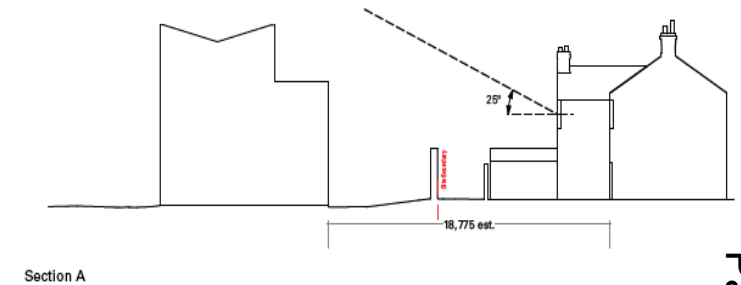
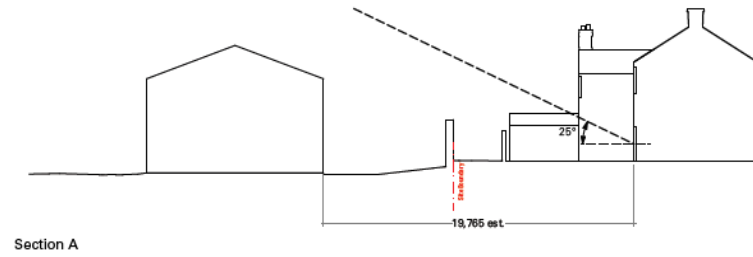
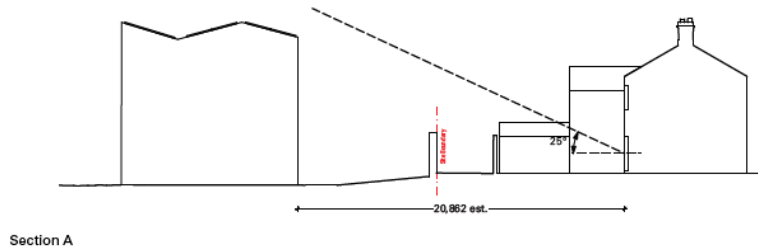
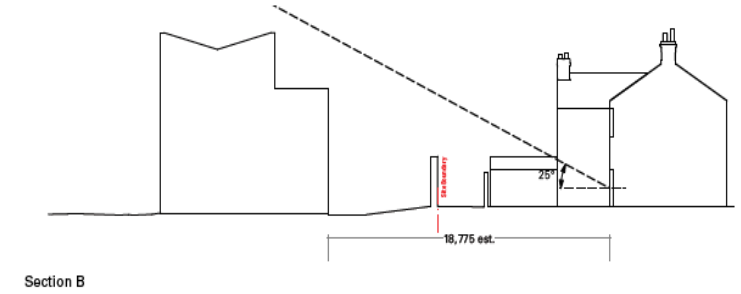
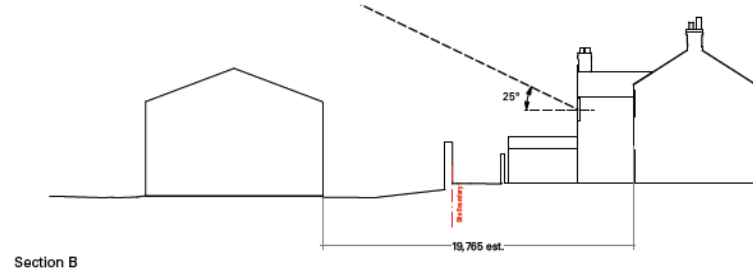
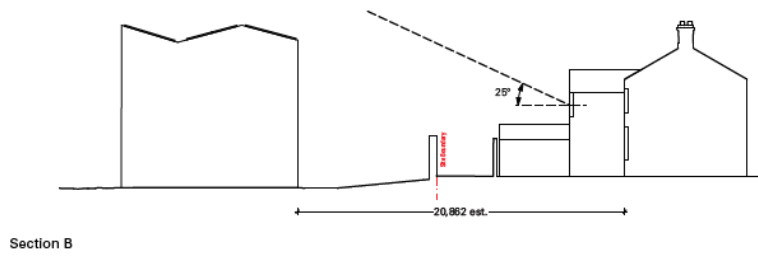
Project Date: **11-11-2021**

Project Name: **1128-URBED-Z0-DR-L-Landscape_Plan**

Author: **1128-URBED-Z0-DR-L-Landscape_Plan**

Date: **11-11-2021**

Version: **P13**



Section Key

Sections to Ambrose Street – Impact Study



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COMMITTEE REPORT**Date:** 9 June 2022**Ward:** Rawcliffe And Clifton
Without**Team:** West Area**Parish:** Rawcliffe Parish Council**Reference:** 22/00192/FUL**Application at:** Clifton Park Treatment Centre NHS North Yorkshire And York
Bluebeck Drive York YO30 5RA**For:** Installation of temporary theatre unit, storage cabin and
additional car parking**By:** Ramsay Health Care UK Ltd**Application Type:** Full Application**Target Date:** 20 April 2022**Recommendation:** Approve**1.0 PROPOSAL**

1.1 The proposal is for a temporary (3 years) operating theatre, storage cabin and additional car parking at Clifton Park Treatment Centre. The scheme is funded by the NHS and seeks to reduce patient waiting lists resulting from the Covid-19 pandemic.

1.2 The new buildings are of modular type construction and will link through to the existing building. They will sit within the existing car park. To accommodate the additional parking requirements, new parking spaces will be formed to the north of the site within an area of open space. The new store is of a similar form to a storage container.

1.3 The site sits within the general extent of the Green Belt. Much of the surrounding land is within Flood Zone 2 but the existing building and parts of the existing car parking are outside of it.

1.4 The existing buildings are a purpose built in-patient hospital (planning application 04/02895/FUL) operated by Ramsay Healthcare. There are 2 operating theatres, 24 beds, a day case unit as well as large out-patients department. Car and cycle parking is provided outside the hospital.

2.0 POLICY CONTEXT

2.1 Section 38(6) of the Planning and Compensation Act 2004 requires that determinations be made in accordance with the development plan unless material considerations indicate otherwise.

2.2 For the purposes of this application the development plan for York comprises the saved policies of the Yorkshire and Humber Regional Spatial Strategy (RSS). These are policies YH9(C) and Y1 (C1 and C2) which relate to York's Green Belt and the key diagram insofar as it illustrates general extent of the Green Belt.

2.3 The saved RSS policies state that the detailed inner and the rest of the outer boundaries of the Green Belt around York should be defined to protect and enhance the nationally significant historical and environmental character of York, including its historic setting, views of the Minster and important open areas.

PUBLICATION DRAFT YORK LOCAL PLAN (2018)

2.4 The Publication Draft Local Plan 2018 (the 'emerging plan') was submitted for examination on 25 May 2018. Phase 1 of the hearings into the examination of the Local Plan took place in December 2019, Phase 2 took place in May 2022. In accordance with paragraph 48 of the NPPF the emerging policies can be afforded weight according to:

- The stage of preparation of the emerging plan (the more advanced the preparation, the greater the weight that may be given);
- The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
- The degree of consistency of the relevant policies in the emerging plan to the policies in the previous NPPF published in March 2012.

2.5 The most relevant policies of the emerging plan are:

SS2	The Role of York's Green Belt
D1	Placemaking
D2	Landscape and Setting
G12	Biodiversity and Access to Nature

GI4	Trees and Hedgerows
GB1	Development in the Green Belt
HW5	Healthcare Services
CC1	Renewable and Low Carbon Energy
CC2	Sustainable Design and Construction of New Development
ENV3	Land Contamination
ENV 4	Flood Risk
ENV5	Sustainable Drainage
T1	Sustainable Access
T7	Minimising and Accommodating Generated Trips

2.6 Although there is no formally adopted local plan the City of York Draft Local Plan Incorporating the Fourth Set of Changes was approved for Development Management purposes in April 2005. It does not form part of the statutory development plan and its policies carry very limited weight.

2.7 The most up-to date representation of relevant policy is the National Planning Policy Framework 2021 (NPPF). Local planning authorities should seek to approve applications for sustainable development where possible and work with applicants to secure developments that improve the economic, social and environmental conditions of the area.

3.0 CONSULTATIONS

Rawcliffe Parish Council

3.1 Object as a result of the loss of amenity space and impact on local residents and dog walkers from the development of the car park. The Parish Council consider that the scheme with new high galvanised fence looks unsightly.

Public protection

3.2 Provision of 2 electric vehicle charging bays and potential for 2 more in the future if required should be secured via condition.

Design, conservation and sustainable development (landscape)

3.3 An area Tree Preservation Order covers the site and a large tract of land beyond it. The trees affected by the proposal are unlikely to be subject to this order due to their relatively young age. Some trees are to be removed to enable the development. Two silver birch trees to be removed are of good quality they are not the main landscape feature which is a group of silver birch trees just beyond the

boundary and unaffected by the application. A horse chestnut, also to be removed, is not in good condition and could be replaced, while a flowering cherry plum is attractive but quick growing and could readily be replaced. Mitigating planting could be achieved and secured via condition.

3.4 The area of open grassland with scrub and trees around the perimeter, to the north of the treatment centre, is recognised as 'amenity' open green space in the CYC open space audit and therefore makes a valid contribution to the recreational value of the local green infrastructure. There is little curtilage to the north of the existing building and therefore the change in character from built form to meadow is quite abrupt. The new fence line would increase the curtilage and provide opportunity to create a softer transition. Despite this the proposal is for car parking which would bring the developed character further into the natural open space.

3.5 The meadowland to the north of the site provides a natural environment with a series of informal paths linking Shipton Road and Rawcliffe Meadows. The proposed car park extension would result in the removal of some tree cover, and would bring the hospital grounds to within very close proximity to the southern footpath. Due to the open character of the land, the development is also exposed to view from the other two routes. The main visual impact would be experienced by way of a loss to the depth of the natural environment and by bringing development of an urban nature closer to the eye. From Shipton Road there is good screening from the existing hedgerow and tree line. The existing fencing around the site is relatively subtle as a result of its grey colour, proposed fencing could copy this. Similarly lighting is proposed and will bring development further into the meadow however light is already emitted from the building and existing car park so additional impacts will be small.

3.6 Space for soft landscaping on site is minimal. The surrounding meadow appears to be within the ownership of the applicant and there could be scope for some limited tree planting here to reduce visual impact of the development.

Design, conservation and sustainable development (Ecology)

3.7 Conditions are recommended to provide biodiversity enhancements in line with the submitted Preliminary Ecological Appraisal and protect nesting birds.

Highways network management

3.8 The proposed car parking exceeds the guidance levels by 5 spaces overall. Given the location of this site, objections are not raised to this application on these

grounds. Conditions are recommended to secure cycle parking, a construction management plan and implementation of a travel plan.

4.0 REPRESENTATIONS

Neighbour notification and publicity

4.1 One letter of support received from the chief executive of York NHS Trust outlining the urgent need for the facility to deal with backlogs of patients waiting for surgery.

4.2 Three letters of objection have also been received. These raise the following concerns:

- Work has already started on site
- Impact on wildlife habitats
- Consultation with residents has not been undertaken
- Impact of lighting for car park on wildlife
- Insufficient screening of car park
- Materials and colour of car park fencing
- Noise from mobile unit
- Refuse bins should be of a colour to blend in with the landscape
- Local environment is being eroded

5.0 APPRAISAL

MAIN ISSUES

- 5.1 Green Belt
- Landscape and visual impact
 - Highways considerations
 - Biodiversity
 - Flood risk and drainage
 - Other issues
 - Very special circumstances

GREEN BELT

5.2 For the purposes of s.38(6) Planning and Compulsory Purchase Act, the proposals should be assessed against the saved RSS Green Belt policies. Policies

contained within the National Planning Policy Framework are also material considerations.

5.3 The 2005 DCLP shows the site, and wider Clifton Hospital area, as Green Belt with Shipton Road forming the boundary. In contrast, recent amendments to the emerging Local Plan (Topic paper 1 Green Belt Addendum January 2021) show the developed area of Clifton Hospital to be outside the Green Belt with the boundary following the developed area. This results in part of the site (that occupied by the existing Clifton Park Treatment Centre and parking) as outside the Green Belt and the area to the north (proposed for new car parking) as within the Green Belt.

5.4 It is the Local Planning Authority's position that until a Local Plan for the City of York is adopted, development management decisions relating to proposals falling within the general extent of the Green Belt are made on the basis that the land should be treated as Green Belt. Therefore Green Belt policies set out within the NPPF apply to the determination of this development proposal.

5.5 Paragraph 147 of the NPPF states: 'Inappropriate development is, by definition, harmful to the Green Belt and should not be approved except in very special circumstances'. Paragraph 148 goes on to state: 'When considering any planning application, local planning authorities should ensure that substantial weight is given to any harm to the Green Belt. 'Very special circumstances' will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm resulting from the proposal, is clearly outweighed by other considerations'.

5.6 Paragraph 149 of the NPPF states: 'A local planning authority should regard the construction of new buildings as inappropriate in the Green Belt. The proposed temporary operating theatre would not meet any of the defined exceptions set out within paragraph 149 of the NPPF. The new parking area represents a material change of use in the land and falls to be considered under paragraph 150 of the NPPF where such changes are appropriate provided they preserve openness and do not conflict with Green Belt purpose. The scheme should however be taken as a whole and, as a result the proposals would, in this context, be considered to amount to inappropriate development within the Green Belt.

IMPACT ON THE OPENNESS OF THE GREEN BELT

5.8 As set out in Paragraph 137 of the NPPF. One of the essential characteristics of Green Belts are their openness and their permanence. There is no definition of 'openness' in the NPPF. However it is commonly taken to mean the state of being free from development, the absence of buildings and relates to the quantum and extent of development and its physical effect on the site. Guidance in the National Planning Policy Guidance indicates that factors which can be taken into account when considering openness include, but are not limited to:

- Spatial and visual aspects
- Duration of the development and its remediability
- Degree of activity likely to be generated.

5.9 Policy GB1 of the emerging plan states that permission will only be granted for development where:

- i. The scale, location and design of development would not detract from the openness of the Green Belt;
- ii. It would not conflict with the purposes of including land within the Green Belt; and
- iii. It would not prejudice or harm those elements which contribute to the special character and setting of York.

5.10 There are unresolved objections to Policy GB1 that will be considered through the examination in public of the Local Plan and therefore it should only be afforded limited weight in the decision making process for the purposes of this application.

5.11 The site is currently partly occupied by a healthcare facility with associated parking and to the north is open meadow. The introduction of a temporary building within the existing built up part of the site would reduce openness both spatially and visually but this impact would be relatively small given the existing buildings on site. Impacts on openness from the proposed car parking to the north would be greater given that this land is undeveloped. Impacts would be reduced by the proximity to existing development, relatively secluded location, and temporary nature of the proposal. It is noted that the proposals are temporary and provision can be secured via condition to return the land to its previous state (ie car parking and meadow).

IMPACT ON GREEN BELT PURPOSES

5.13 The proposed development would be inappropriate development in the Green Belt. It would lead to a degree of harm to the openness of the Green Belt. Paragraph 138 of the NPPF sets out that the Green Belt serves five purposes. These are:

- a) To check the unrestricted sprawl of large built up areas;

- b) To prevent neighbouring towns merging into one another;
- c) To assist in safeguarding the countryside from encroachment;
- d) To preserve the setting and special character of historic towns; and
- e) To assist in urban regeneration, by encouraging the recycling of derelict land and other urban land.

5.14. The primary purpose of the York Green Belt is to safeguard the special character and setting of the historic city as referred to in Policy YH9C of the RSS and Policy SS2 of the 2018 emerging local plan, although weight can only be attached to the latter. Topic Paper 1: Approach to defining York's Green Belt Addendum (2021) identifies that the developed area of the site serves no Green Belt purpose however the open land to the north is important for preserving the setting of the historic city, preventing unrestricted sprawl and safeguarding the countryside from encroachment. Development on this site would therefore cause harm to Green Belt purpose.

LANDSCAPE AND VISUAL IMPACT

5.15 The proposed operating theatre buildings and store would have little visual impact as they would be viewed in the context of the existing buildings.

5.16 The new car park area however introduces development into an area recognised as amenity open space and this land therefore makes a valid contribution to the recreational value of the local green infrastructure. While the existing building has little curtilage to the north resulting, currently, in a sharp change of character between the built development and open space, the new scheme will bring development further into the natural open space.

5.17 Despite this, views of the development will be limited. There are no formal public rights of way within the open space although a series of informal paths criss-cross the area connecting Shipton Road and Rawcliffe Meadows. These informal paths will be very close to the new car park and clearly visible resulting in a reduction in natural environment. From Shipton Road the existing hedge and trees provide good screen although there will be some oblique views through gaps in vegetation and during the winter months.

5.18 The proposed fencing is not overly prominent in the landscape and is appropriate. Similarly, lighting columns are proposed which would bring development further into the space particularly during the hours of darkness in the

winter. However light is already emitted from the existing buildings and car park and the additional impact would be small.

5.19 The proposed car park results in the loss of a small number of semi-mature trees of species which can be readily replaced if appropriate. The two main groups of silver birch (one within the site and one just outside) are to be retained. The proposal includes the creation of a native hedgerow to the north and east of the new car park.

HIGHWAYS CONSIDERATIONS

5.20 There are currently 54 car parking spaces (30 staff, 21 visitors and 3 disabled) on site including. The proposal results in the provision of 38 staff parking spaces, in the new car park to the north, 5 staff parking spaces to the west of the site, 25 visitor parking spaces and 3 disabled spaces. This is an over-provision of 1 space which is not considered significant given this location.

5.21 There are 10 existing bike stands on site providing parking for 20 bicycles. There is visitor cycle parking adjacent to the entrance and further covered cycle stands for staff to the west of the building. There is sufficient space around the visitor cycle parking to accommodate a range of bicycles. Submission of an updated travel plan prior to commencement, and annually reviewed, could identify whether further cycle parking was required following implementation of the proposals.

BIODIVERSITY

5.22 A preliminary ecological appraisal has been submitted to support the application. This discusses the need for biodiversity enhancements in accordance with paragraph 174d of the NPPF with an aim to providing biodiversity net gain. A biodiversity enhancement plan has been submitted and includes provision of a native hedge around the north and east of the car park, bat boxes, hedgehog holes in fencing and bird nesting boxes. These recommendations are secured via condition. The scheme would as a result be in accordance with the NPPF and policy GI2 of the emerging Local Plan.

5.23 There is potential for nesting birds on site there precautionary methods are recommended where vegetation removal is required during the nesting bird season. These are to be secured via condition.

FLOOD RISK AND DRAINAGE

5.24 The proposed car park is within Flood Zone 2 although the hospital itself is predominantly in Flood Zone 1. NPPF paragraph 168 notes that applications for minor development (non-residential extensions with a footprint less than 250m²) and changes of use do not require a sequential or exception test but should still meet the requirements of site specific flood risk assessments. The site is protected by flood defences.

OTHER ISSUES

5.25 A number of the issues raised in the representations have been covered above and are addressed by planning conditions. The scheme has been assessed by the Council's Ecologist and appropriate mitigation is considered to be secured.

5.26 Officers are aware that the temporary building is now on site and that work has begun on the construction of the car park to the rear. The applicant has been informed that any work undertaken prior to receiving planning consent takes place at their own risk, and attention has been particularly drawn to their responsibilities to nesting birds under the Wildlife and Countryside Act 1981.

VERY SPECIAL CIRCUMSTANCES

5.25 The proposed development represents inappropriate development in the Green Belt. Paragraph 147 of the NPPF explains that inappropriate development is, by definition, harmful to the Green Belt and should not be approved except in very special circumstances. Paragraph 148 says when considering any planning application, local planning authorities should ensure that substantial weight is given to any harm to the Green Belt. 'Very special circumstances' will not exist unless the potential harm to the Green Belt by reason of inappropriateness and any other harm resulting from the proposal, is clearly outweighed by other considerations.

5.26 In this instance harm has been identified through inappropriateness, as well as some impact on openness and harm to Green Belt purpose.

5.27 The following considerations have been put forward to justify the proposal:

The proposal has solely been generated as a result of demand from the NHS due to a backlog of operations resulting from the Covid 19 pandemic. Clifton Park Hospital has a shortage of operating theatre space and its viability as a continuing health care provider is at risk if more cannot be provided. The hospital has been used much more by the NHS recently and this has heightened the demand for short-term residential health care on site. The initial request is for a temporary period of 3 years.

The new building will provide for the vast majority of orthopaedic elective surgery in the city with the remaining capacity being used to create additional day case capacity for urology patients. The theatre will enable the NHS Trust to protective surgical elective capacity from Covid-19 surges and winter pressures.

The new building will be sited within an area identified as not serving any Green Belt purpose and therefore removed from the Green Belt following adoption of the emerging Local Plan.

6.0 CONCLUSION

6.1 The above report outlines how the proposed development, subject to conditions, can be compliant with the NPPF with regards to impacts upon the highway network, sustainable travel, biodiversity, flood risk and drainage.

6.2 At present the site is considered to be within the general extent of the Green Belt. As a result the proposal is considered inappropriate by definition. Further harm has been identified as a result of the impact on openness of the new building and the proposed car park. The car park introduces development, albeit in a relatively modest form, into an area previously undeveloped and will therefore cause harm to the Green Belt purpose of safeguarding the countryside from encroachment.

6.3 Paragraph 148 of the NPPF requires that substantial weight is given to any harm to the Green Belt. 'Very special circumstances' will not exist unless potential harm to the Green Belt by reason of inappropriateness, and any other harm is clearly outweighed by other considerations. These other considerations take the form of a

known and urgent need for additional operating theatre capacity to address NHS waiting lists as a result of the Covid 19 pandemic. It is considered that this provides the justification to clearly outweigh the harm through inappropriateness, and other identified harm, as required by paragraph 148. The application is recommended for approval subject to planning conditions.

7.0 RECOMMENDATION: Approve

1 The development or works hereby permitted shall be discontinued and the land reinstated to its former condition at or before 3 years from the date of this decision.

Reason: To preserve Green Belt purpose and openness.

2 The development hereby permitted shall be carried out in accordance with the following plans and other submitted details:-

Location plan

Proposed site plan 6687-P04 F

Proposed ground floor plan 6687-P05 E

Proposed elevations 6687-P06 A

Portastore elevations PSN6-171

Tree protection plan BA11219TPP P

Biodiversity enhancement plan (Dryad Ecology Dated May 2022)

Bird protection plan

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

3 The building shall not be occupied until the areas shown on the approved plans for parking and manoeuvring of vehicles (and cycles, if shown) have been constructed and laid out in accordance with the approved plans, and thereafter such areas shall be retained solely for such purposes.

Reason: In the interests of highway safety.

4 No works shall take place until a Construction Traffic Management Plan (CTMP) has been submitted to and approved in writing by the Local Planning Authority. The CTMP shall include but not be limited to the following information:

- where constructing over existing tarmacked areas, showing how the existing building will be served without disruption to the highway network.
- measures to prevent the egress of mud and other detritus onto the adjacent public highway;
- a dilapidation survey jointly undertaken with the local highway authority
- the routing for construction traffic that will be promoted;
- a scheme for signing the promoted construction traffic routing;
- where contractors will park
- hours of operation (inc. deliveries);
- where materials will be stored within the site and
- contact details for the main contractor

Works shall be subsequently implemented in accordance with the approved details.

Reason: To ensure that the development can be carried out in a manner that will not be to the detriment of amenity of local residents or the safe and free passage of highway users

5 Protection of existing trees shown to be retained shall be in strict accordance with the approved Tree Protection Plan.

Reason: To safeguard the welfare of existing trees that are protected by a tree preservation order and/or make a significant contribution to the amenity of the development and/or area.

6 No vegetation removal shall take place between 1st March and 31st August inclusive, unless a competent ecologist has undertaken a careful, detailed check of vegetation for active birds' nests immediately before the works and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation should be submitted to the local planning authority before such works commence

Reason: To ensure that nesting birds are protected from harm during construction. All British birds, their nests and eggs (with certain limited exceptions) are protected by Section 1 of the Wildlife and Countryside Act 1981, as amended.

7 Development shall be carried out in accordance with the recommendations of the Biodiversity Enhancement Plan (Dryad Ecology Dated May 2022) and any mitigation required by that plan shall be completed no later than the end of the first winter dormancy period following issue of this decision. Maintenance shall be undertaken in accordance with the schedules contained within the Biodiversity Enhancement Plan.

Reason: To take account of and enhance the biodiversity and wildlife interest of the area, and to be in accordance with Paragraph 174 d) of the NPPF (2021) to

contribute to and enhance the natural and local environment by minimising impacts on and providing net gains for biodiversity, including establishing coherent ecological networks that are more resilient to current and future pressures.

8 Within 3 months of the date of this decision a Travel Plan, based on the '2021 - 2022 Travel Plan Clifton Park Hospital', shall be submitted and approved in writing by the LPA. The Travel Plan should be developed and implemented in line with local and national guidelines. The site shall thereafter be occupied in accordance with the aims, measures and outcomes of the approved Travel Plan.

Within 12 months of occupation of the site a first year travel survey shall have been submitted to and approved in writing by the LPA. Results of yearly annual travel surveys, carried out for the 3 years of the development, shall then be submitted annually to the authority's travel plan officer for approval.

Reason: To ensure that traffic flows from the site can be safely accommodated and to promote the usage of sustainable means of transport.

9 Within 30 months of the date of this decision a revised biodiversity enhancement plan shall be submitted and approved in writing by the Local Planning Authority. The revisions will address the methods proposed, timescales and maintenance schedules for the reinstatement of the car park site to its original habitat. The approved details shall be fully implemented and maintained in accordance with the approved details.

Reason: To take account of and enhance the biodiversity and wildlife interest of the area, and to be in accordance with Paragraph 174 d) of the NPPF (2021) to contribute to and enhance the natural and local environment by minimising impacts on and providing net gains for biodiversity, including establishing coherent ecological networks that are more resilient to current and future pressures.

10 Prior to occupation, a "lighting design strategy for biodiversity" for the new car park shall be submitted and approved in writing by the Local Planning Authority. The strategy shall:

a) identify those areas/features on site that are particularly sensitive for bats and that are likely to cause disturbance along important routes used to access key areas of their territory, for example, for foraging; and

b) show how and where external lighting will be installed (through the provision of appropriate lighting contour plans and technical specifications) so that it can be clearly demonstrated that areas to be lit will not disturb or prevent the above species using their territory or having access to their breeding sites and resting places.

All external lighting shall be installed in accordance with the specifications and

locations and timescales set out in the approved strategy, and these shall be maintained thereafter in accordance with the strategy. Under no circumstances should any other external lighting be installed without prior consent from the local planning authority.

Reason: To ensure that the introduction of artificial light does not mean that protected species active at night (bats) are disturbed or discouraged from using areas of existing habitat.

8.0 INFORMATIVES:

Notes to Applicant

1. STATEMENT OF THE COUNCIL'S POSITIVE AND PROACTIVE APPROACH

In considering the application, the Local Planning Authority has implemented the requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application. The Local Planning Authority took the following steps in order to achieve a positive outcome:

Requested further information in relation to biodiversity impacts

2. In the UK, due to the decline in bat numbers in the last century, all species of bat are protected by the Wildlife & Countryside Act (1981) as amended, Countryside and Rights of Way Act (2000) and the Conservation of Habitats and Species Regulations 2017 (as amended). Planning consent for a development does not provide a defence against prosecution under this act. Because of their protected status, it should be noted that if bats are discovered during the course of the work, all works must cease and Natural England must be informed immediately. It is an offence for anyone to disturb or handle a bat without the appropriate licences. This may cause some delay but should not prevent the work continuing, provided that due account is taken of their presence.

When designing external lighting its potential impacts on light sensitive species, such as bats, should be taken into account. Advice on lighting design for light sensitive species is available from the Bat Conservation Trust (2018) Bats and artificial lighting in the UK guidance:

<https://cdn.bats.org.uk/pdf/Resources/ilp-guidance-note-8-bats-and-artificial-lighting-compressed.pdf?mtime=20181113114229&focal=none>

3. The applicant is advised to consider using permeable fencing or leaving occasional gaps suitable to allow passage of hedgehogs. Any potential hibernation sites including log piles should be removed outside the hibernation period (which is between November and March inclusive) in order to avoid killing or injuring

hedgehog.

Hedgehogs are of priority conservation concern and are a Species of Principal Importance under section 41 of the NERC Act (2006). An important factor in their recent population decline is that fencing and walls are becoming more secure, reducing their movements and the amount of land available to them. Small gaps of approximately 13x13cm can be left at the base of fencing to allow hedgehogs to pass through. Habitat enhancement for hedgehogs can easily be incorporated into developments, for example through provision of purpose-built hedgehog shelters or log piles.

<https://www.britishhedgehogs.org.uk/wp-content/uploads/2019/05/developers-1.pdf>

4. The applicant is reminded that, under the Wildlife and Countryside Act 1981, as amended (section 1), it is an offence to remove, damage or destroy the nest of any wild bird while that nest is in use or being built. Planning consent for a development does not provide a defence against prosecution under this act. Suitable habitat is likely to contain nesting birds between 1st March and 31st August inclusive. As such habitat is present on the application site and is to be assumed to contain nesting birds between the above dates, unless a recent survey has been undertaken by a competent ecologist to assess the nesting bird activity on site during this period and has shown it is absolutely certain that nesting birds are not present.

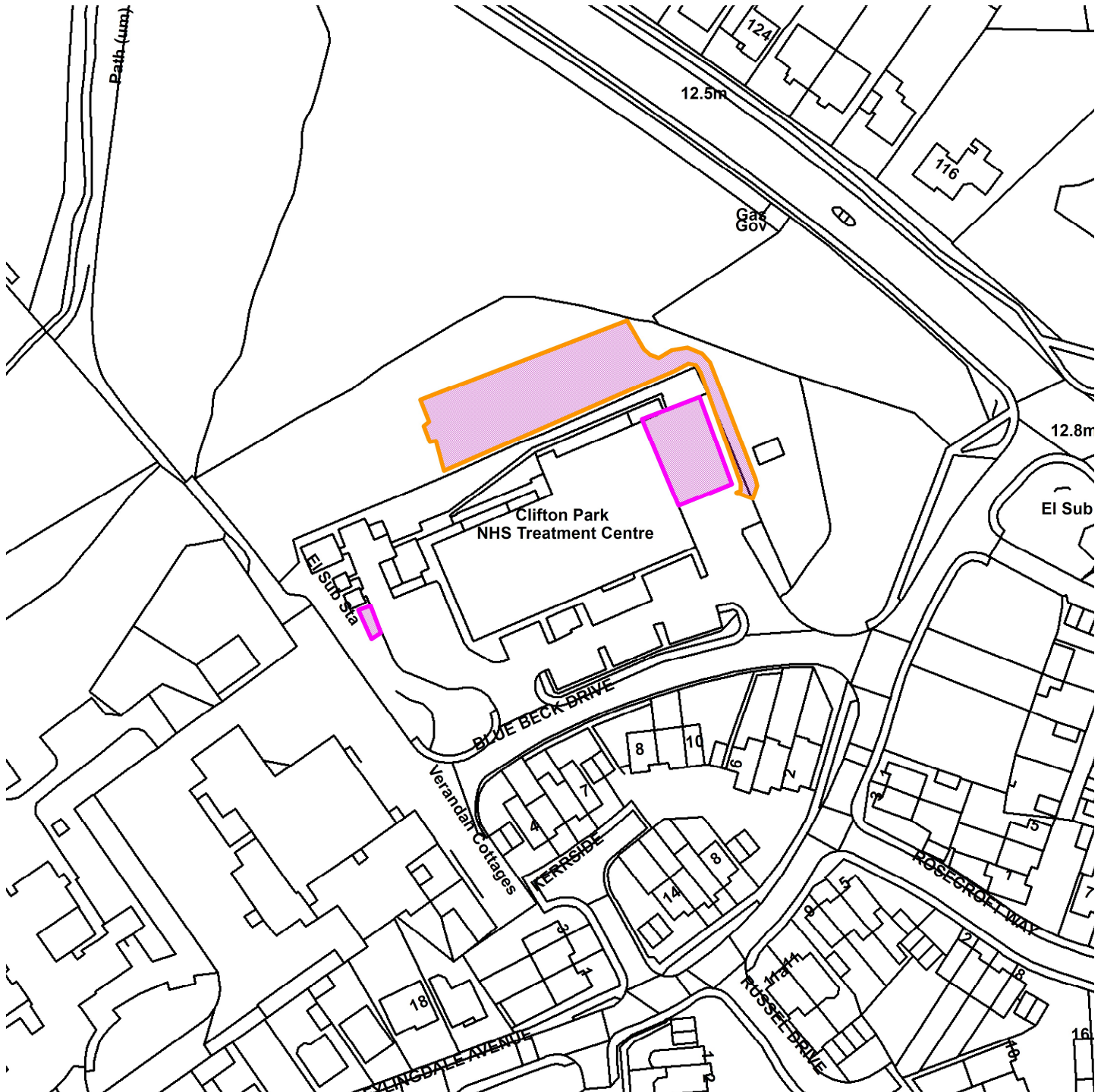
Contact details:

Case Officer: Alison Stockdale

Tel No: 01904 555730

22/00192/FUL

Clifton Park Treatment Centre, Bluebeck Drive, YO30 5RA



Scale : 1:1305

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Organisation	City of York Council
Department	Directorate of Place
Comments	Site location plan
Date	26 May 2022
SLA Number	

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Planning Committee A

To be held on Thursday 9th June 2022 at 4:30pm

22/00192/FUL – Clifton Park Treatment Centre, NHS North Yorkshire and York, Bluebeck Drive, York

Installation of temporary theatre unit, storage cabin and additional car parking

Access to new car park



Rear Boundary



Siting for new building



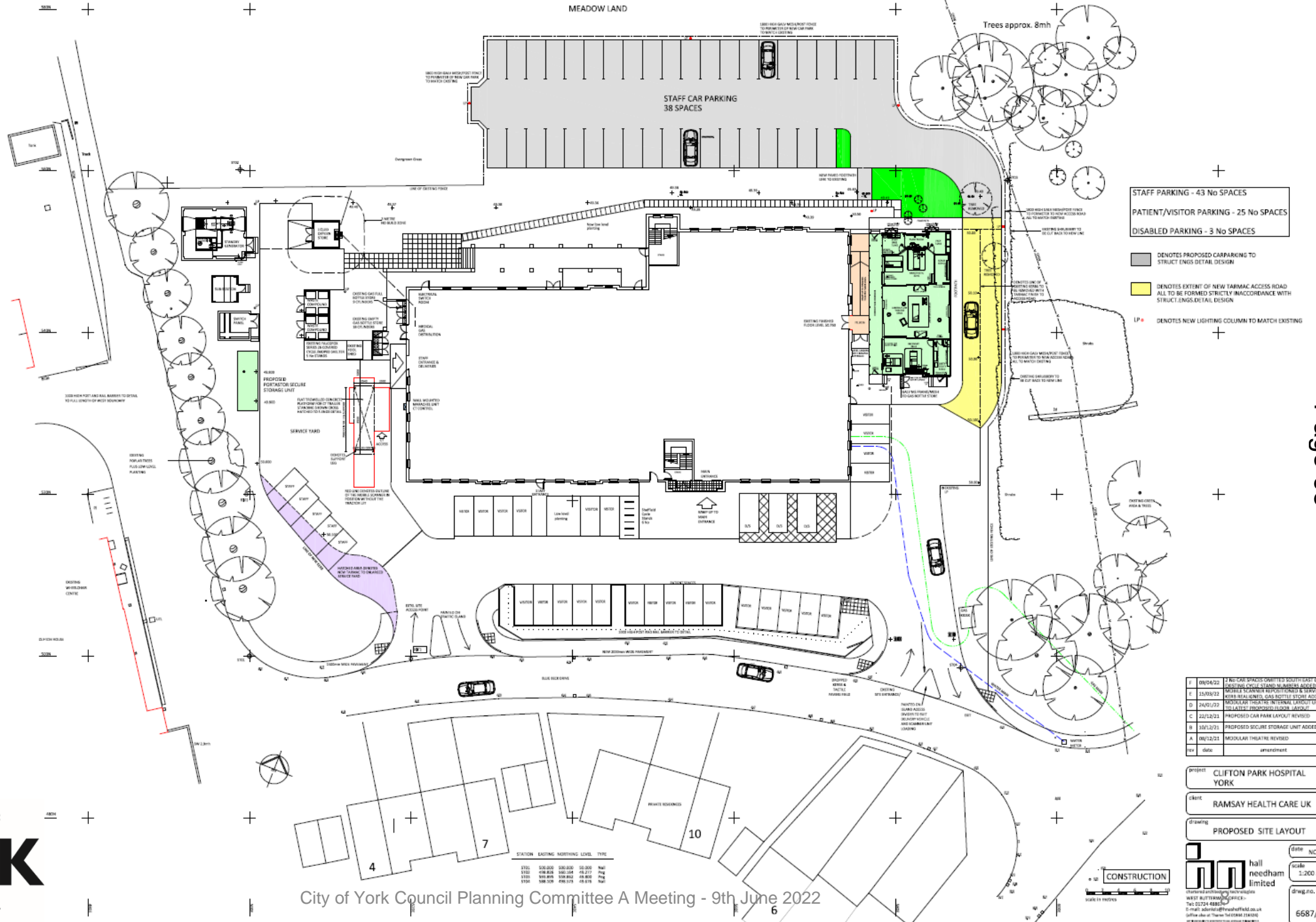
Siting for new car park



Siting for new store



Proposed Site Plan



STAFF PARKING - 43 No SPACES
 PATIENT/VISITOR PARKING - 25 No SPACES
 DISABLED PARKING - 3 No SPACES

■ DENOTES PROPOSED CARPARKING TO STRUCT ENGS DETAIL DESIGN
 ■ DENOTES EXTENT OF NEW FARM ACCESS ROAD ALL TO BE FORMED STRICTLY IN ACCORDANCE WITH STRUCT ENGS DETAIL DESIGN
 LP ■ DENOTES NEW LIGHTING COLUMN TO MATCH EXISTING

F	09/04/22	3 NO CAR SPACES OBTAINED ADDITIONAL CORNER DETENTION CYCLE STANDS TO BE ADDED
F	15/09/22	REVISED SCANNING APPROVALS & SUPPLY TRAIL VIEWS REALIGNED GAS BOTTLE STORE ADDED
D	24/01/22	MODULAR THEATRE INTERNAL LAYOUT UPDATED TO INCLUDE PROPOSED LUNDSHAW ROADWAY
C	22/12/21	PROPOSED CAR PARK LAYOUT REVISED
B	16/12/21	PROPOSED SECURE STORAGE UNIT ADDED
A	08/12/21	MODULAR THEATRE REVISED
REV	DATE	AMENDMENT

Project: CLIFTON PARK HOSPITAL YORK
 Client: RAMSAY HEALTH CARE UK

Drawing: PROPOSED SITE LAYOUT

Scale: 1:200 at A1
 Date: NOV 21
 Drawing No: 6687/P04

hall needham limited
 Chartered architectural practitioners
 15-17, Colton Road, York YO1 1JH
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 Email: admin@hallneedham.co.uk
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City of York Council Planning Committee A Meeting - 9th June 2022

Proposed Elevations



SOUTH ELEVATION



NORTH ELEVATION



EAST ELEVATION



City of York Council Planning Committee A Meeting - 9th June 2022

A	08/12/21	MODULAR THEATRE REVISED
rev	date	amendment

project	CLIFTON PARK HOSPITAL YORK
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client	RAMSAY HEALTH CARE UK
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drawing	PROPOSED ELEVATIONS
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date	NOV 21
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scale	1:200 at A3
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drwg.no.	rev.
6687/P06	A

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Planning Committee A

9 June 2022

Planning Committee B

8 June 2022

Planning Appeal Performance and Decisions

- 1 This report informs Members of planning appeal decisions determined by the Planning Inspectorate between 1 January and 31 March 2022. A summary of each decision is provided in appendix B and a list of outstanding planning appeals in appendix C.
- 2 Appeal statistics are collated by the Planning Inspectorate on a quarterly and annual basis. The Government use the statistical returns as one of a number of measures to assess the performance of local planning authorities. To assess the quality of decisions, this is based on the total number of decisions made by the Local Planning Authorities that are subsequently overturned at appeal. The threshold whereby a Local Planning Authority is eligible for designation as under-performing is 10% of the Authority's total number of decisions on major, non-major and "county-matter" (generally minerals and waste proposals) applications made during the assessment period being overturned at appeal.
- 3 Table 1 shows results of planning appeals decided by the Planning Inspectorate for the quarter 1 January to 31 March 2022 for all types of planning appeals such as those against the refusal of planning permission, against conditions of approval, listed building applications and lawful development certificates.

Table 1: CYC Planning Appeals Last Quarter Performance

	01/01/22 to 31/03/22
Allowed	5
Split decision	0
Dismissed	10
Total Decided	15
% Allowed	33%

- 4 There were no appeal decisions received during the quarter relating to the refusal of an application for a “major” development.

Table 2: CYC Planning Appeals 12 month Performance

	01/04/21 to 31/03/22	01/04/2020 to 31/03/21
Allowed	7	10
Split decision	1	3
Dismissed	33	41
Total Decided	40	54
% Allowed	17%	18.5%

- 5 For the 12 months period 1 April 2021 to 31 March 2022, 17% of CYC appeals decided were allowed. Nationally, provisional figures show that 25% of appeals were allowed over the same period.
- 6 Using the assessment criteria set out in paragraph 2 above, 0.4% of the total CYC decisions made in respect of non-major applications and 0% of total decisions made in respect of major applications in the period 1 April 2021 – 31 March 2022 were overturned at appeal. There were no appeals in respect of county-matter applications during the period. These figures are provided for illustrative purposes, the DLUHC collate figures over a rolling two year period.
- 7 A list of the planning appeals determined between 1 January and 31 March 2022 are included in Appendix A. Summaries of the decisions are included in Appendix B. One no. appeal was determined following a decision to refuse permission made by the sub-committee/committee.

Table 3: Appeals Decided 01/01/2022 to 31/03/2022 following Refusal by Committee / Sub-Committee

Ref No	Site	Proposal	Appeal Outcome
20/01498/ FUL	The Jubilee PH, Balfour Street	Conversion of roofspace and part first floor to 3no. flats, retention of public house on ground floor and part first floor	Allowed

- 8 The list of current appeals is attached at Appendix C. There are 19 appeals of all types awaiting determination.

Consultation

- 9 This is an information report for Members and therefore no consultation has taken place regarding its content.

Council Plan

- 10 The report is relevant to the “Well-paid jobs and an inclusive economy,” “Creating homes and World-class Infrastructure,” “A Greener and cleaner city,” “Getting around sustainably” and “Good Health and Wellbeing” city outcomes of the Council Plan.

Implications

- 11 Financial – There are no financial implications directly arising from the report.
- 14 Human Resources – There are no Human Resources implications directly involved within this report and the recommendations within it other than the need to allocate officer time towards the provision of the information.
- 15 Legal – There are no known legal implications associated with this report or the recommendations within it.
- 16 There are no known Equalities, Property, Crime & Disorder or other implications associated with the recommendations within this report.

Risk Management

- 17 In compliance with the Council’s risk management strategy, there are no known risks associated with the recommendations of this report.

Recommendation

- 19 That Members note the content of this report.

Reason

- 20 To inform Members of the current position in relation to planning appeals against the Council’s decisions as determined by the Planning Inspectorate.

Contact Details

Author:

Gareth Arnold
Development Manager,
Development Management

Chief Officer Responsible for the report:

Becky Eades
Head of Planning and Development
Services

Report
Approved



Date 25.05.2022

Specialist Implications Officer(s) None.

Wards Affected:

All Y

For further information please contact the author of the report.

Appendices

Appendix A – Planning Appeals decided between 1 January and 31 March 2022

Appendix B - Summaries of Planning Appeals decided between 1 January and 31 March 2022

Appendix C - Outstanding Planning and TPO Appeals as at 25 May 2022

Appendix A

Planning Appeals decided between 1 January and 31 March 2022

App number	Proposal	Site	Decision	Officer rec	Rec'n Acc'd	Decision type
20/01474/LBC	Single storey rear extension	44 Clifton York YO30 6AW	Appeal Dismissed	Refuse	Accepted	Delegated
21/01503/FUL	Dormer to rear and 2no. rooflights to front	Rushbrooke 21A Holyrood Drive York YO30 5WB	Allowed	Refuse	Accepted	Delegated
21/01387/FUL	Variation of condition 2 of permitted application 21/00199/FUL to reduce height of extension	30 Cherry Wood Crescent York YO19 4QN	Appeal Dismissed	Refuse	Accepted	Delegated
20/01498/FUL	Conversion of roof space and part of first floor to 3no. flats and retention of public house on ground floor and part first floor (resubmission).	The Jubilee Balfour Street York YO26 4YU	Allowed	Approve	Overtaken - refused	Sub Committee
21/01719/FUL	Two storey side and single storey rear extension	33 Walney Road York YO31 1AH	Appeal Dismissed	Refuse	Accepted	Delegated
21/01670/FUL	Rendering of Eastfield Crescent elevation	20 Eastfield Crescent York YO10 5JB	Allowed	Refuse	Accepted	Delegated
21/01339/PAAA	Enlargement of dwellinghouse by construction of 1no. additional storey with new roof height no greater than 3.5 metres above the highest part of the existing roof under Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) - Schedule 2, Part 1,	The Lodge Westerly Lake Main Street Wheldrake York YO19 6AH	Allowed	Refuse	Accepted	Delegated

	Class AA					
21/00612/FUL	Erection of stable block, associated yard area and access track (resubmission)	2 Scoreby Lane Scoreby York YO41 1NW	Appeal Dismissed	Refuse	Accepted	Delegated
21/00692/FUL	Single storey side/rear extension in connection with use of house as a C4 House in Multiple Occupation - resubmission	15 Yarburgh Way York YO10 5HD	Appeal Dismissed	Refuse	Accepted	Delegated
21/01901/FUL	Single storey side and rear extension - Costs Refused 21.3.22	46 Markham Street York YO31 8NR	Appeal Dismissed	Refuse	Accepted	Delegated
21/00194/FUL	Erection of detached dwelling with associated access, landscaping and air source/ground source heat pump.	Proposed Residential Development Site Shilton Garth Close Earswick York	Allowed	Refuse	Accepted	Delegated
21/01614/FUL	Replacement of existing timber single glazed vertical sliding sash windows with uPVC double glazed vertical sliding sash windows to front and rear elevations	18 Priory Street York YO1 6EX	Appeal Dismissed	Refuse	Accepted	Delegated
21/01211/FUL	Single storey rear extension	9 New Lane Bishopthorpe York YO23 2QS	Appeal Dismissed	Refuse	Accepted	Delegated
19/01454/FUL	Proposed change of use from office to residential to include demolition and replacement of existing rear extension and installation of dormer to rear	Mudd And Co 5 Peckitt Street York YO1 9SF	Appeal Dismissed	Refuse	Accepted	Delegated
19/01455/LBC	Proposed internal and external alterations in association with change of use from office to residential to include demolition and replacement of existing rear extension, installation of dormer to rear, reinstatement of internal wall and repositioning of doors	Mudd And Co 5 Peckitt Street York YO1 9SF	Appeal Dismissed	Refuse	Accepted	Delegated

Appendix B

Summaries of Planning Appeals decided between 1 January and 31 March 2022

Case number	Description	Address	Outcome
21/00037/REF	Dormer to rear and 2no. rooflights to front	Rushbrooke 21A Holyrood Drive York YO30 5WB	Appeal Allowed

Notes

The application was refused on the grounds that the rear dormer by virtue of its size and design was detrimental to the character and appearance of the host property and the character of the area. The Inspector considered that whilst dormers are not an established feature of the rear roofscape there is a notable absence of rhythm due to the presence of mixed roof forms and differing alignment of properties which provide opportunity for alterations without causing harm to character and distinctiveness of local area. The Inspector notes that the Householder SPD does not seek to prevent rear dormers even where there are no rear dormers and considers that the dormer is not overly large or dominant and would therefore not have an unacceptable impact on the property or group of properties from public vantage points.

Case number	Description	Address	Outcome
21/00038/REF	Variation of condition 2 of permitted application 21/00199/FUL to reduce height of extension	30 Cherry Wood Crescent York YO19 4QN	Appeal Dismissed

Notes

The application was refused on the grounds of harm to visual amenity, the roof design of the proposed extension would be at odds with the form of the main roof in a prominent position within the streetscene. The extension would have an undue prominence and appear awkward, incongruous and out of keeping with the appearance of the dwelling. The extension would project beyond the building line established by nearby properties. The Inspector dismissed the appeal, agreeing with the Council as to the incongruity of the proposed extension. They did not find harm in regard to the building line on this part of Cherry Wood Crescent, as the extension would significantly exceed it and a similar projection had already been approved. They considered there to be some

variety in the building line on this part of the street.

Case number	Description	Address	Outcome
21/00041/REF	Alterations and conversion of part of first floor and all of the roofspace of the public house building to 3no. self-contained apartments and retention of public house on ground floor and altered function room on first floor (resubmission).	The Jubilee Balfour Street York YO26 4YU	Appeal Allowed

Notes

Sub Committee Decision – officer recommendation to Approve. The appeal related to the refusal of the change of use of part of the vacant and dilapidated Jubilee Public House to three flats, retaining part of the function room on the first floor and the majority of the pub on the ground floor along with a beer garden. The application was refused primarily because it was considered that the loss of staff accommodation and the introduction of self contained flats would undermine the viability of the pub and lead to the loss of part of the pubs community provision. A similar proposal was refused in 2018 and dismissed at appeal. The main change between the two applications was that the current appeal scheme retained a function room on the first floor. The Inspector allowed the appeal stating that the proposed changes to the pub would not unacceptably harm what could be provided for the local community. In assessing the proposal the Inspector agreed with the previous Inspector that there was nothing compelling relating to the proposed changes that would mean that the pub could not remain viable. The Inspector included a number of conditions on the decision including the need for the pub areas to be fully restored prior to the occupation of the second flat and removing permitted development rights to subdivide the reformatted function room or use it for ancillary living accommodation.

Case number	Description	Address	Outcome
21/00044/REF	Two storey side and single storey rear extension	33 Walney Road York YO31 1AH	Appeal Dismissed

Notes

The application was refused on the grounds of harm to visual amenity, with the scale and position of the proposed extension in

relation to the side boundary and the adjacent street constituting an overbearing presence and eroding the spacing within the streetscene, of specific importance given the prominence of the corner site. The design of the extension was also considered to be out of keeping with the distinctive gable ended design of the host dwelling, constituting an addition of unsympathetic design in a prominent location within the streetscene. The Inspector dismissed the appeal, finding that the proposed side extension would be a substantial addition which would unbalance the existing symmetry of the property. No examples of a similar scale were found in the surrounding area, and the scheme was considered to be highly visible given its location, serving to erode the characteristic relationship between properties and their plots that prevails in this part of the street. The Inspector fully agreed with the Council as to the incongruity of the scheme, and found that the proposed hipped roof would interact awkwardly with existing roof forms, appearing unsympathetic in relation to the host property and discordant in the wider area.

Case number		Description	Address	Outcome
21/00039/REF		Enlargement of dwellinghouse by construction of 1no. additional storey with new roof height no greater than 3.5 metres above the highest part of the existing roof under Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) – Schedule 2, Part 1, Class AA	The Lodge Westerly Lake Main StreetWheldrakeYorkYO19 6AH	Appeal Allowed

Notes

The appeal related to a prior approval application. The application was refused because the extra storey would result in a development that would visually encroach into open land around Wheldrake in contrast to the discreet and unassuming appearance of the existing dwelling. The property is located within the Green Belt, however, the prior approval process does not allow the impact on Green Belt openness to be assessed. The Inspector allowed the appeal, stating that the dwelling would be more visible in the landscape, but did not consider that this would detract from the setting of the village. In considering this, the Inspector gave regard to the existing trees that partly screened the building, its separation from agricultural land and the existence of views where the dwelling would be seen adjacent to development in the village.

Case number	Description	Address	Outcome
21/00036/REF	Erection of stable block, associated yard area and access track (resubmission)	2 Scoreby Lane Scoreby York YO41 1NW	Appeal Dismissed

Notes

The application was refused because it was inappropriate development in the Green Belt. The Planning Inspector agreed that the proposal failed to preserve the openness of the Green Belt and would fail to safeguard the countryside from encroachment and as such was inappropriate development in the Green Belt. The Inspector considered that the private benefits to the appellant and the benefits to animal welfare to have limited weight and did not outweigh the harm to the Green Belt

Case number	Description	Address	Outcome
21/00034/REF	Erection of detached dwelling with associated access, landscaping and air source/ground source heat pump.	Proposed Residential Development Site Shilton Garth Close Earswick York	Appeal Allowed

Notes

The proposal related to the erection of 1no. 4-bedroom dwelling on land within the built up area of Earswick village. Planning permission was refused due to the conflict with policies ENP1 and ENP2 of the Earswick Neighbourhood Plan, which seek to meet the needs of the parish for smaller 2 and 3 bedroom homes. Outline planning permission for the erection of two dwellings on the site had been granted in 2018. The key issue identified by the Inspector was whether the proposed development would provide adequate accommodation to meet local housing needs. The Inspector acknowledged the conflict with policies ENP1 and ENP2, but stated that the threshold would only be exceeded by one bedroom and in respect of a single dwelling. He noted that the parish council had indicated that the policy was not created with the intention of precluding the development of single dwellings. He considered that the development would deliver social and economic benefits, and the sustainable from of construction would result in environmental benefits. In addition, the Council could not demonstrate a 5-year supply of housing land. Crucially, the Inspector referred to paragraph 14 of the NPPF, which indicates that in relation to the provision of housing, a Neighbourhood Plan (NP) only carries weight if it formed part of the Development Plan for a period of two years or less prior to the decision being made. Whilst this was the case when the decision was made by the Council, the two year period had lapsed at the point the appeal was

determined, thus the NP was deemed to be out of date in this respect. In allowing the appeal, the Inspector concluded that the provision of a 4-bedroom dwelling rather than a 2 or 3 bedroom dwelling would result in limited harm, which would be outweighed by the benefits of the proposal. He concurred with the Council that the other impacts of the development were acceptable.

Case number	Description	Address	Outcome
21/00028/REF 21/00029/REF	Proposed change of use from office to residential to include demolition and replacement of existing rear extension and installation of dormer to rear Planning and Listed Building Consent applications	Mudd And Co5 Peckitt StreetYorkYO1 9SF	Appeal Dismissed

Notes

The proposal involved a change of use from office to residential, part removal and rebuild of existing substandard rear kitchen, installation of dormer to rear roof plane and internal alterations. Planning and Listed Building Consent applications were submitted. It was refused on the grounds that the existing rear extension was of important heritage and aesthetic value and demolition would result in harm to the significance. No convincing evidence had been provided to indicate that the structure was at risk of collapse. The applicant suggested that the site is at risk from flooding to justify removal but the Council's Senior Flood Risk Engineer advised that this was not the case. The existing extension adds greatly to the character of the listed building, whereas the proposed extension was notably taller and wider and would appear awkward and the proposed rear dormer would create an incongruous feature within the roofscape. The details of the proposed internal door on the ground floor were considered to be crude and the loss of an existing on the second floor would mean that the original circulation pattern could not be read. The Inspector considered that the removal of the rear extension would result in the loss of features of special interest that contributed to the overall significance of the listed building and would cause clear harm to the heritage asset. The proposed replacement would be of a greater height and width and would be conspicuous and appear as a discordant addition thereby diminishing its significance. The proposed rear dormer window was clumsy and would harm the character and appearance of the conservation area. In terms of flooding there was little evidence that the identified deficiencies could not be adequately addressed by a comprehensive scheme of repairs by a suitably qualified conservation specialist. With regard to the internal doors these were minor matters that could be dealt with via condition. No evidence presented to suggest that the property would become vacant in the event that the appeal failed

Case number	Description	Address	Outcome
21/00043/REF	Single storey rear extension	9 New Lane Bishopthorpe York YO23 2QS	Appeal Dismissed

Notes

The planning appeal related to the refusal of householder application 21/01211/FUL for a single storey rear extension. The proposed extension was considered to have a detrimental impact on the character and appearance of both the host dwelling and the row of terraced properties it forms part of. In particular, the design and length of the extension was deemed to be at odds with the main house and it was felt that the extensions overall appearance did not respect the existing dwelling or the character of the area. In addition to this, the extension was also considered to cause a detrimental impact on the amenity of the occupiers of both No.8 and No.10 New Lane with regards to loss of outlook and dominance. The extension was deemed to be overbearing and oppressive due to its length and height along the shared boundaries. The Inspector dismissed the appeal agreeing with both reasons for refusal.

Case number	Description	Address	Outcome
21/00035/REF	Rendering of Eastfield Crescent elevation	20 Eastfield Crescent York YO10 5JB	Appeal Allowed

Notes

The application was for light-brown render to be applied to the north elevation of a two storey semi-detached dwelling. The application was refused on the grounds of harm to visual amenity, the proposed render being considered incongruous and out of keeping with the brick-built character of the host dwelling, its semi-detached partner and other dwellings along Eastfield Crescent. The Inspector allowed the appeal. They acknowledged that the proposed rendering would be a change in finish to the existing brickwork of No 18 and No 20, but judged that the proposed light brown (mink) colour would be sympathetic and not dissimilar in colour to the existing brickwork of the 2 dwellings. So although different, the proposal would not be incongruous in character or

appearance and would not result in visual harm. The 'key determining factor' in the Inspector's decision was the similarity in colour between the render and the existing brickwork. Because of this, they considered the proposal not to be contrary to the expectations of the 'House Extensions and Alterations' SPD, and consistent with the NPPF's requirements for developments to be sympathetic to local character and of good design, despite the render being 'different' to the existing character.

Case number	Appeal by	Description	Address	Outcome
21/00030/REF	Mr And Mrs Cleaver	Single storey side/rear extension in connection with existing use of house as a C4 House in Multiple Occupation - resubmission	15 Yarburgh Way York YO10 5HD	Appeal Dismissed

Notes

The application was refused on the grounds that the proposal would result in a significant expansion of the HMO without properly addressing issues relating to car and cycle parking and refuse bin storage.

In dismissing the appeal the Inspector stated that the parking fell substantially short of the 3.6 metre width the Council sought to achieve and there would therefore be insufficient space to facilitate the day-to-day practicalities of loading and unloading that comes with the more intense use generated by residents of a HMO. In addition the configuration of the parking layout, was not practical, as it would be extremely difficult for vehicles to park in the manner proposed. He commented that the surrounding streets are already heavily parked and it was likely that these parking pressures would be further exacerbated at peak times of the day owing to the proximity of the nearby school and parade of shops. The proposal made no provision for secure cycle storage or parking and there was no space to the side of the property to allow for the passage of cycles and refuse bins from the rear of the property to its front. Consequently, it was likely that wheeled refuse bins, recycling boxes and cycles would be stored to the front of the property. Not only was this likely to discourage a sustainable form of transport, but this would also introduce unsightly clutter to the front of the property, impeding the proposed parking layout and harmful to the character and appearance of the area.

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Appendix C
Outstanding Planning and TPO Appeals

Date report run: 25-May-2022

Ward	Appeal Case number	Proposal	Address	Officer name
Copmanthorpe	22/00011/REF	Conversion of 2no. buildings to form holiday letting accommodation with alterations to existing vehicular access (resubmission, revised scheme)	125 Temple Lane Copmanthorpe York YO23 3TE	Erik Matthews
Dringhouses And Woodthorpe	22/00010/REF	Erection of 60no. retirement apartments with care, communal facilities, parking, landscaping and associated amenity space following demolition of existing 3no. bungalows.	1 Cherry Lane York YO24 1QH	Erik Matthews
	22/00018/REF	Single storey side extension	3 The Grove York YO24 1XD	Matthew Parkinson
Guildhall	22/00017/REF	Rebuilding of roof with dormer to rear, 3no. rooflights to front and 1 lantern light to ridge	8 Portland Street York YO31 7EH	David Johnson
Haxby And Wigginton	22/00012/REF	Conversion of existing house into two dwellings with single storey rear extension, side and rear dormers, rear extension, demolition of garage and erection of new dwelling to rear	Cedar House 29 Station Road Haxby York YO32 3LU	Matthew Parkinson
Heworth	22/00003/REF	Erection of 1no. dwelling to rear of No.62 Heworth Road and conversion of outbuilding to dwelling with single storey extension following demolition of detached garage (resubmission)	62 Heworth Road York YO31 0AD	Simon Glazier
	22/00004/NON	Change of use from dwelling house	28 Heworth York YO31	Paul Edwards

		(use class C3) to House in Multiple Occupation (use class C4)	1AF	
Hull Road	21/00050/TPO	Fell 1no. Cedar; crown lift Lime tree by 2m protected by Tree Preservation Order No.: 52	The Quadra Garrow Hill York YO10 3HL	Brian Williams
Huntington/New Earswick	21/00032/NON	Outline planning permission with all matters reserved except access, for circa 300 residential dwellings, associated landscaping, public open space, and the formation of two new vehicle accesses from New Lane	Huntington South Moor New Lane Huntington York	Jonathan Kenyon
	21/00033/NON	Outline planning application with full details of means of access for residential development of circa 970 dwellings with associated demolition, infrastructure works, open space, primary school, community facilities and convenience store on land west of Monks Cross Link Road and a country park with drainage infrastructure east of Monks Cross Link Road	Site To The West Of The A1237 And South Of North Lane Huntington York	Neil Massey
	22/00015/NON	Erection of 8no. dwellings with associated works following demolition of existing buildings	The Wilberforce Trust Wilberforce House 49 North Moor Road Huntington York YO32 9QN	Victoria Bell
Micklegate				
	21/00045/NON	Erection of extra care accommodation including no.70 apartments and decked car park with associated private amenity space, landscaping, substation and vehicular access alterations	Chocolate Works Residents Parking Bishopthorpe Road York	Erik Matthews

	21/00051/HH	High hedge investigation	Field House 2 St Georges Place York YO24 1DR	Ed Bainbridge
	22/00006/CON	Installation of louvres, 2no. doors and external Amazon lockers, provision of plant and machinery and changes to previously approved store opening and delivery hours.	David Wilson Homes Limited Marketing Suite Hallmark House Joseph Terry Grove York YO23 1PX	Erik Matthews
	22/00009/REF	Change of Use from Class C3 Dwellinghouse to Serviced Holiday Apartment and installation of new external door to rear elevation at basement level	75 The Mount York YO24 1AX	Erik Matthews
Rawcliffe And Clifton Without	22/00005/REF	Two storey extension to side and rear with canopy porch to front (revised scheme, resubmission)	9 Holyrood Drive York YO30 5WB	Sam Baker
	22/00007/REF	Two storey side extension	2 Hendon Garth York YO30 5ZB	Joseph Bourke
Rural West York	22/00013/TPO	Fell Monkey Puzzle tree protected by Tree Preservation Order no. CYC9	23 Nursery Road Nether Poppleton York YO26 6NN	Brian Williams
	22/00014/REFC PD	Certificate of lawfulness of proposed development comprising: removal of existing railway carriage; erection of workshop/wood store, potting shed and boundary fencing; and construction of swimming pool	Cherry Tree Cottage Millfield Lane Nether Poppleton York YO26 6NX	Matthew Parkinson

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